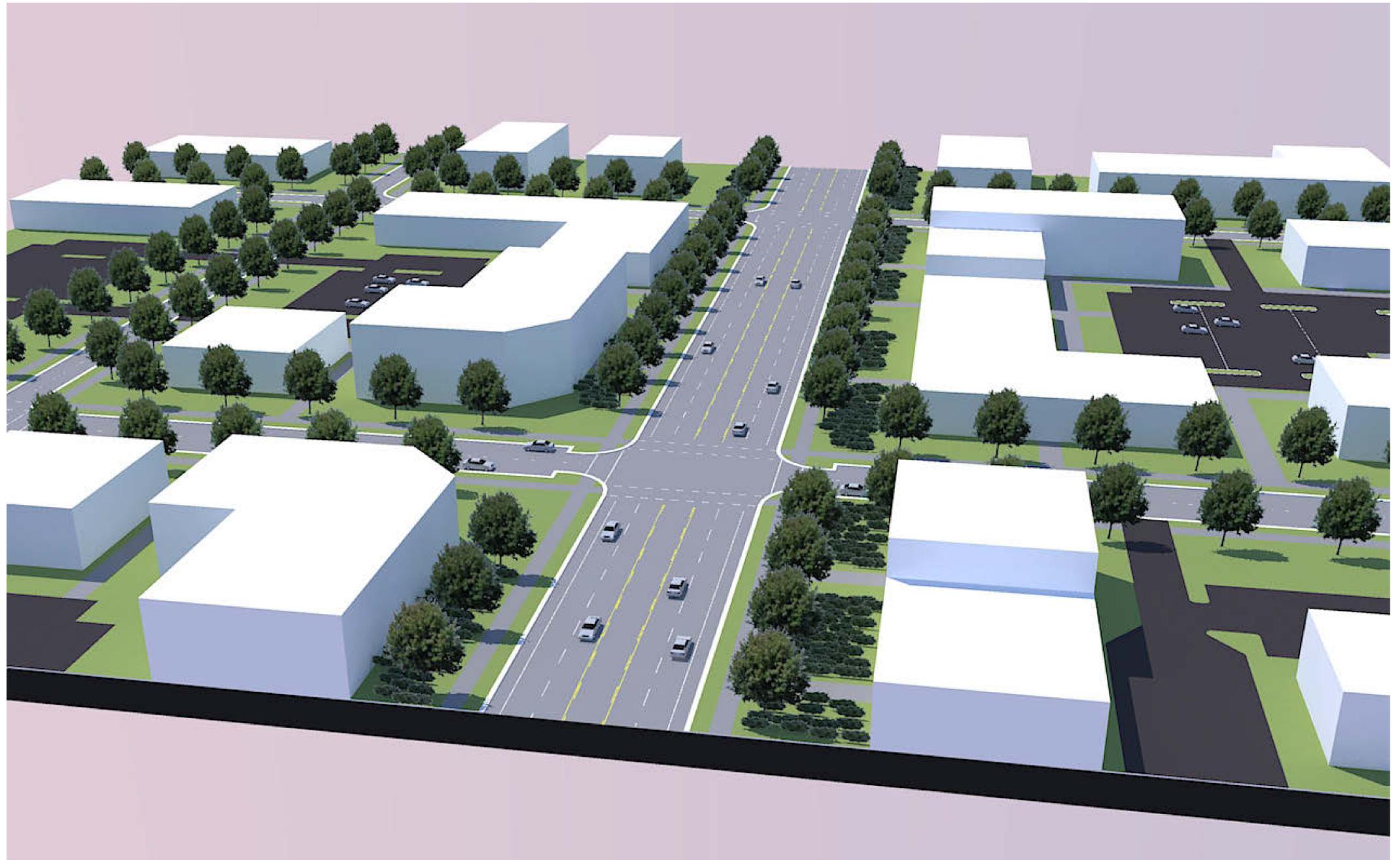


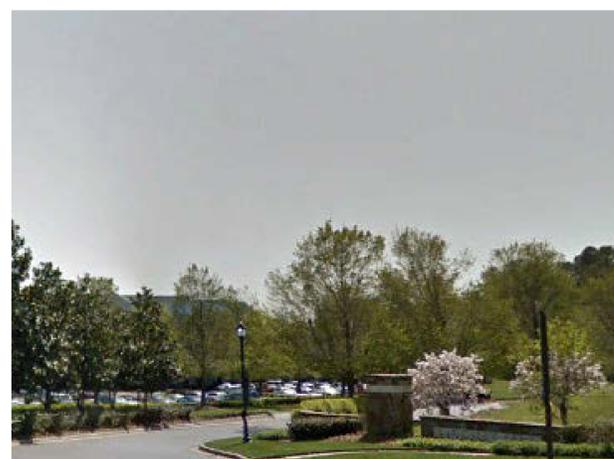
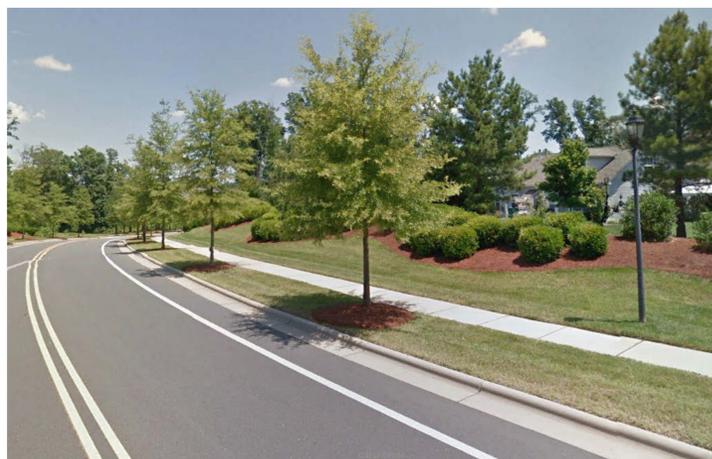
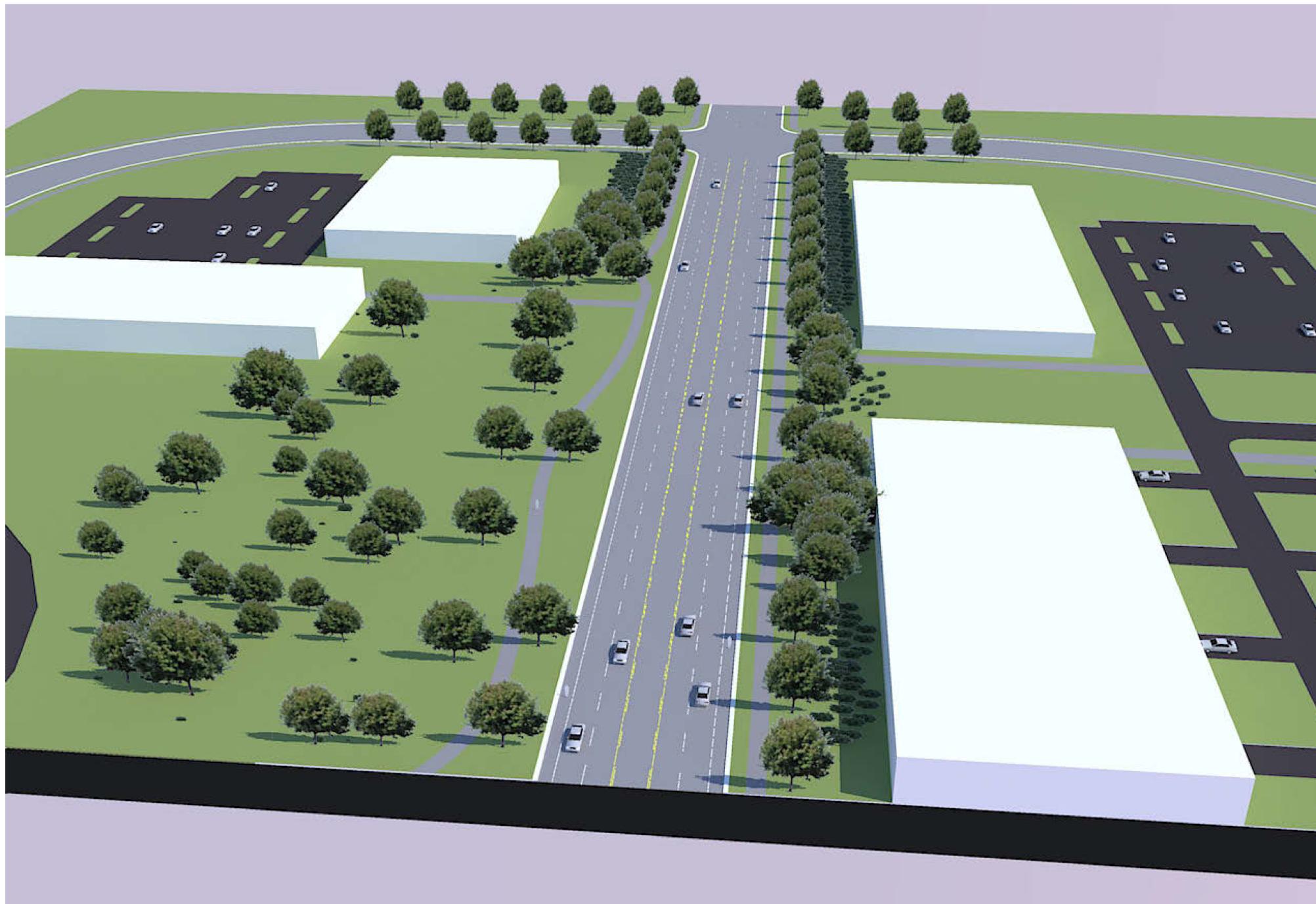
Southern Bypass Corridor Overlay District

Design Concept- Node



Southern Bypass Corridor Overlay District

Design Concept- Other



Southern Bypass Corridor Overlay District

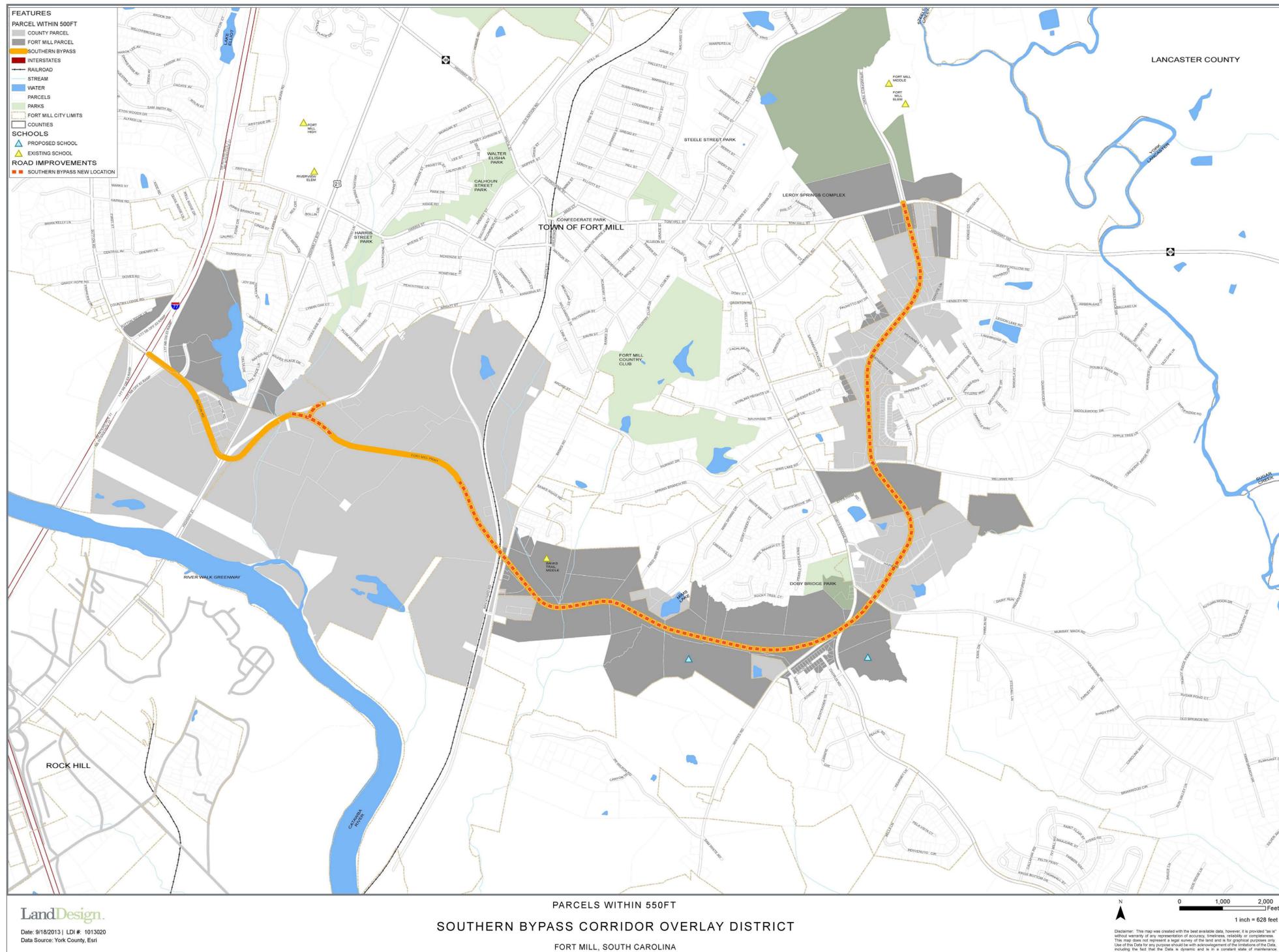
Purpose & Applicability

PURPOSE: The Corridor Overlay District (COD) is established for the purpose of maintaining a safe, efficient, functional and attractive roadway corridor for the Fort Mill Southern Bypass (Bypass). It is recognized that, in areas of high visibility, the protection of features that contribute to the character of the area and enhancements to development quality promote economic development and stability in the entire community.

APPLICABILITY:

Parcels in the COD and the COD-N shall be as designated on the official Zoning Map. Such parcels shall be adjacent to the Bypass right-of-way (Corridor) and wholly or in part within 500 feet of the outer edge of the right-of-way of the Corridor. Where part of a parcel is within 500 of the right-of-way of the Corridor, the entire parcel shall be subject to these regulations, unless specifically excluded below.

The standards established in this section shall be applied to single-family subdivisions (ltd), multifamily residential and all nonresidential development which lies within the area delineated on the Zoning Map.

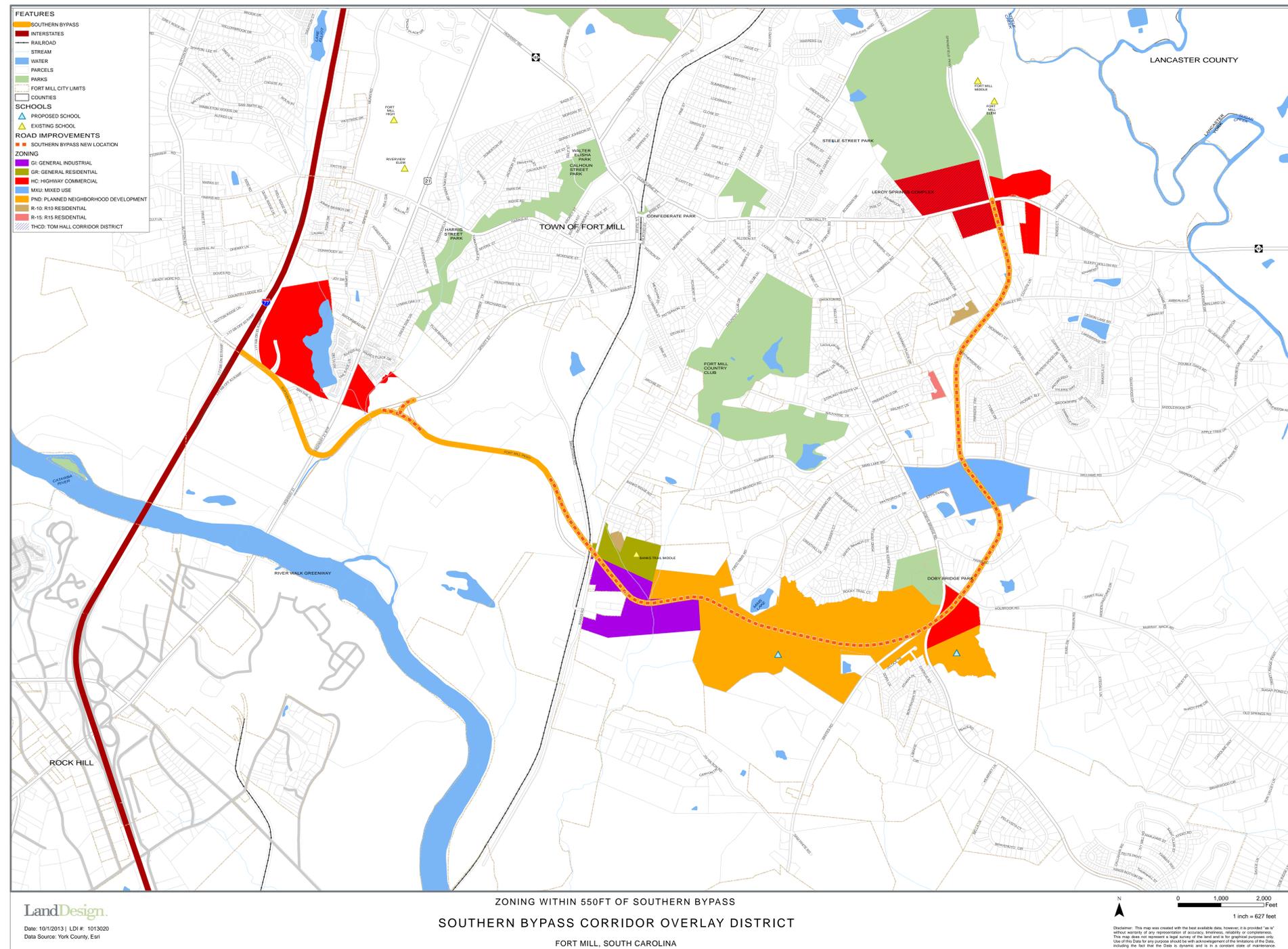


Southern Bypass Corridor Overlay District

Permitted Uses

All permitted, special and conditional uses of the underlying zoning district are allowed subject to the specific requirements and procedures for each use classification, except...

- Automobile rental and sales.
- Automotive wrecker service.
- Casino or gambling establishment.
- Check cashing establishments, title loan lenders, deferred presentment lenders, pawnshops, loan brokers, and small loan companies.
- Communications towers.
- Where such towers must be permitted per the Telecommunications Act of 1996 and it has been demonstrated that no existing towers or structures (such as rooftops, water towers, etc.) can accommodate such equipment, the towers shall not exceed 100 feet in height. To the extent practicable, they shall be roof-mounted, not freestanding, structures.
- Industrial or heavy manufacturing uses (prohibited in COD-N only).
- Junk or salvage yards.
- Mobile homes.
- Sexually-oriented businesses.



Southern Bypass Corridor Overlay District

Dimensional Requirements

The dimensional requirements in the COD and COD-N districts shall be the same as the underlying zoning district, except as follows...

SETBACKS

The building setbacks of the underlying zoning districts shall apply, except along the Corridor. Building setbacks, measured from the ROW, shall be as follows:

	Minimum	Maximum	Average
COD-N	35'	None	45'
COD	50' or 100' (landscape buffer required varies)	None	N/A

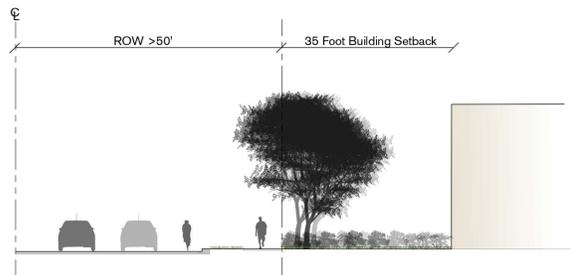
HEIGHTS

The building setbacks of the underlying zoning districts shall apply, except along the Corridor. Building setbacks, measured from the ROW, shall be as follows:

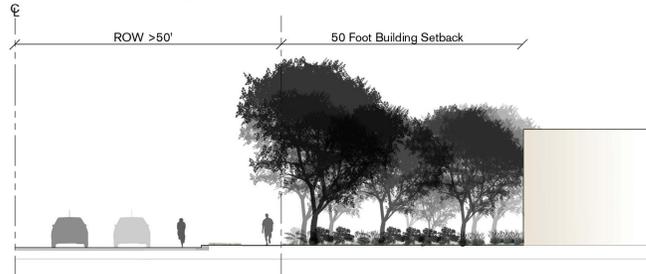
	Minimum	Maximum
COD-N	24'	45'
COD	N/A	Underlying Zoning

Height may be above the maximum height indicated, provided all portions of the structure exceeding the height limit indicated shall be stepped back an additional one foot from the adjoining property for each additional foot in excess.

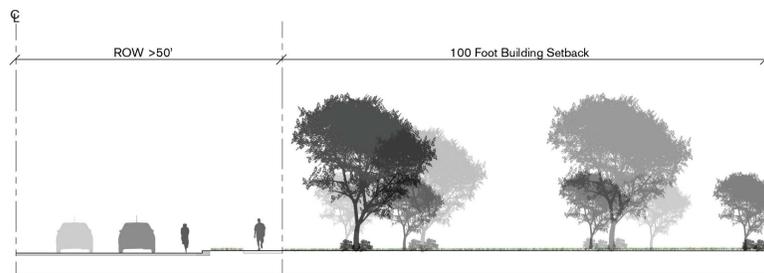
COD-N Typical Building Setback



COD-O 50 Foot Building Setback



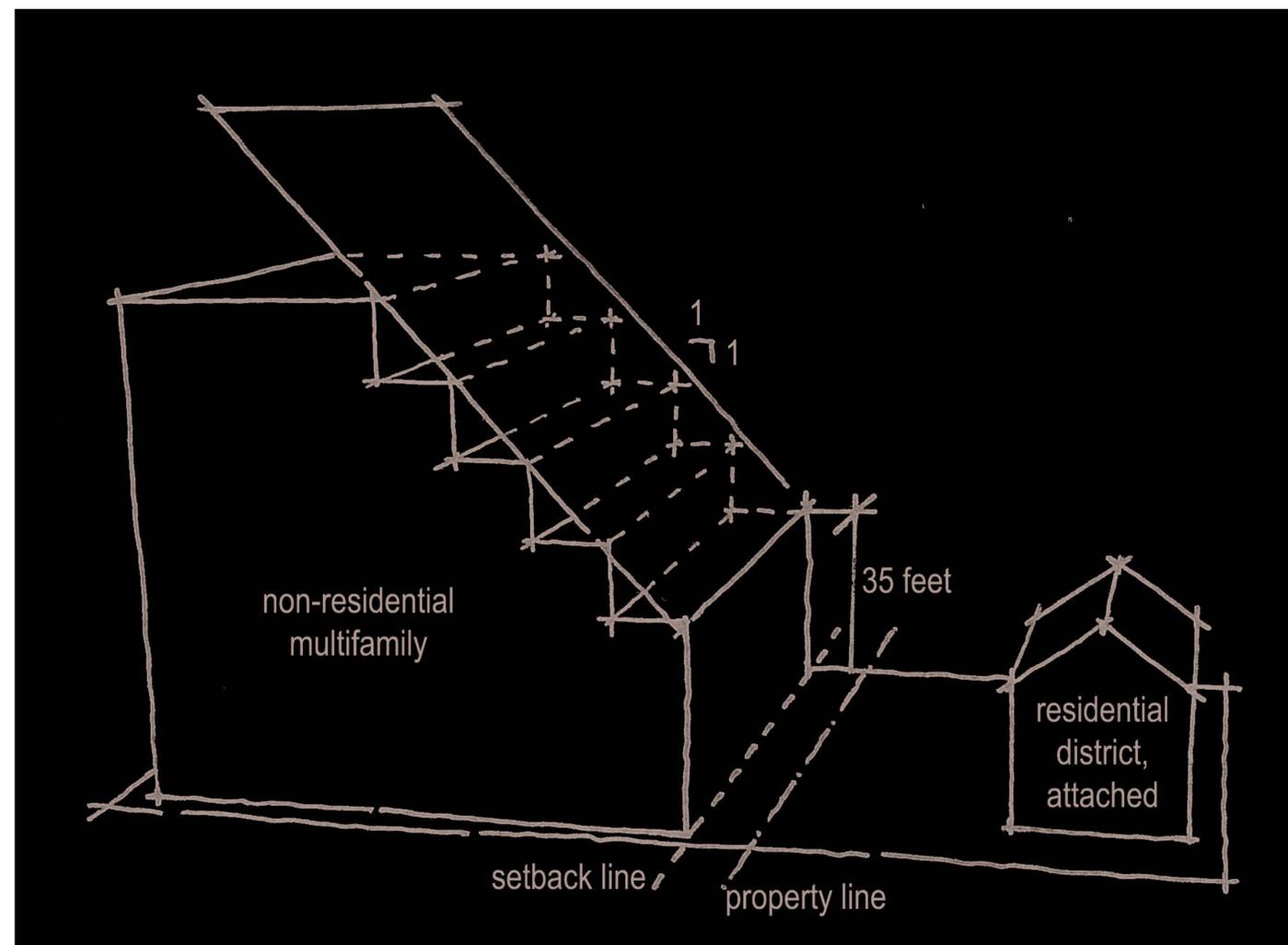
COD-O 100 Foot Building Setback



The maximum setback in COD-N may be exceeded where the area(s) between the building and the setback is utilized for the preservation of significant trees in accordance with Section 3 of Article IV; stormwater management (i.e., detention or retention basins); retaining wall(s), or avoidance of steep topography; formal green space or park located to serve as foreground to the buildings that flank it. The minimum setback in COD shall be reduced in cases where the setback area exceeds 40% of the acreage of a parcel already in existence. The width of the setback area shall be reduced to the extent necessary (up to a 15-foot reduction) in order that the buffer shall not exceed 40% of the parcel. No setback shall be less than 35 feet.

There shall be no development allowed in the setback, except as follows:

- Drainage features;
- Driveways;
- Landscaping;
- Lighting;
- Parks and park-like facilities (not including ball fields);
- Public utilities (limited to lines and other equipment);
- Retaining wall(s);
- Sidewalks, pedestrian ways, or bikeways;
- Signs;
- Streetscape elements.



Southern Bypass Corridor Overlay District

Orientation & Architecture

ORIENTATION

- **The rear facades of buildings in COD shall not be visible from the Corridor. Such facades shall be oriented away from view from the Corridor or shall be screened.**
- **In COD-N, articulation of all non-residential facades, including rear, is required. Therefore, those rear facades do not have to be screened.**
- **Buildings shall be oriented toward the public street(s).**
 - Pedestrian access from the street is encouraged for all multi-family residential and nonresidential uses. Therefore, primary entrances shall be visible and accessible from the public street, where feasible.
 - Where parking is provided at the rear of the building, the primary entrance may be located to provide access from such parking. Two primary entrances, one from the street and one from the rear parking area, are permitted.



ARCHITECTURE

- **Materials:**
 - Rock, stone, brick, stucco, concrete, wood and Hardiplank.
 - No mirrored glass with a reflectance greater than 20 percent shall be permitted.
 - Corrugated metal shall not be used on any façade.
- **In COD-N, any façade facing a street shall be articulated with architectural features and treatments to enhance the quality of pedestrian environment of the public street, particularly in the absence of a primary entrance.**



Southern Bypass Corridor Overlay District

Streetscape

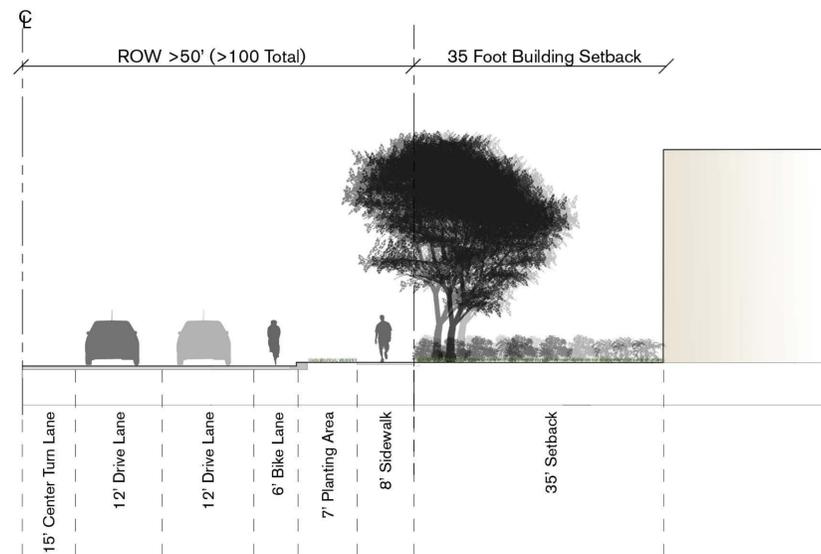
COD-N

Street trees shall consist of canopy trees planted within the streetscape zone (the first 15 feet of the setback) at rate of one tree per 50 linear feet along all Corridor frontages. Tree spacing shall be not more than 60 feet and not less than 40 feet on center. At planting, street trees shall be a minimum of 2" caliper or 8 feet in height.

All new development or expansions of existing development resulting in a minimum ten percent increase in building floor area or lot area shall provide landscaping in setback:

- Trees - for every 2,500 square feet of planting area
 - a minimum of two trees shall be planted
 - at least 50% canopy trees
 - street trees shall be counted
- Shrubs - for every 2,500 square feet of planting area
 - a minimum of ten shrubs shall be planted
 - at least 50% of the shrubs planted shall be evergreen
 - at least 3' at maturity

COD-N Typical Streetscape



Southern Bypass Corridor Overlay District

Buffers & Screening

BUFFERS

Landscaped buffers shall meet the following:

- 20 to 50 feet in width
- Natural, undisturbed
- Trees shall be protected in accordance with Section 3, Article IV, Landscaping Standards.
- Plant for every 100' without vegetation or if disturbed:

<5 acres	<5-10 acres	>10 acres
20 feet	35 feet	50 feet
3 canopy trees	5 canopy trees	5 canopy trees
6 understory	10 understory	10 understory
9 shrubs	15 shrubs	20 shrubs



SCREENING

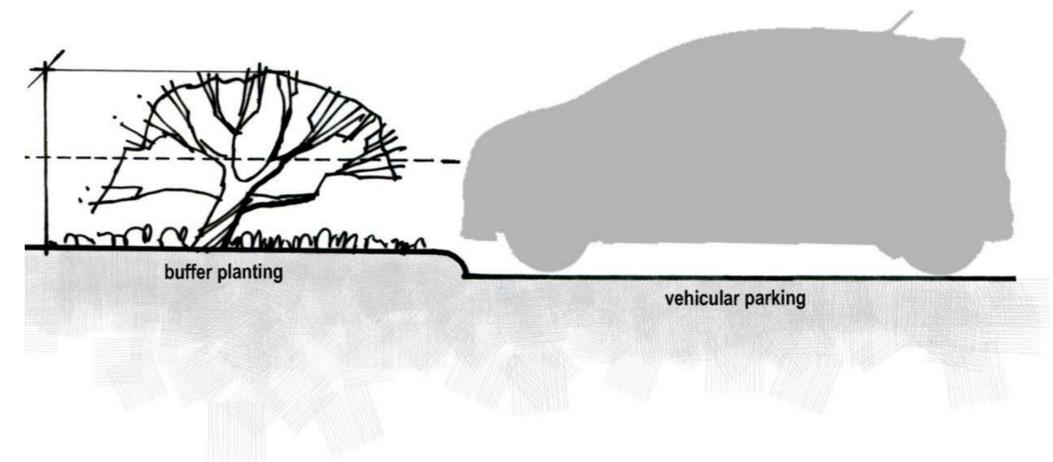
Multifamily and nonresidential development against SF residential:

- Landscaped buffer
- Evergreen trees and shrubs
- required buffers shall be located entirely on the parcel of the developing multifamily and non-residential use

Loading areas and some rear facades in COD – landscaped buffer, fence, wall, or combination

Parking areas of multifamily and nonresidential: shrubs five feet on center, at least three feet in height at maturity

3 feet
at maturity



Southern Bypass Corridor Overlay District

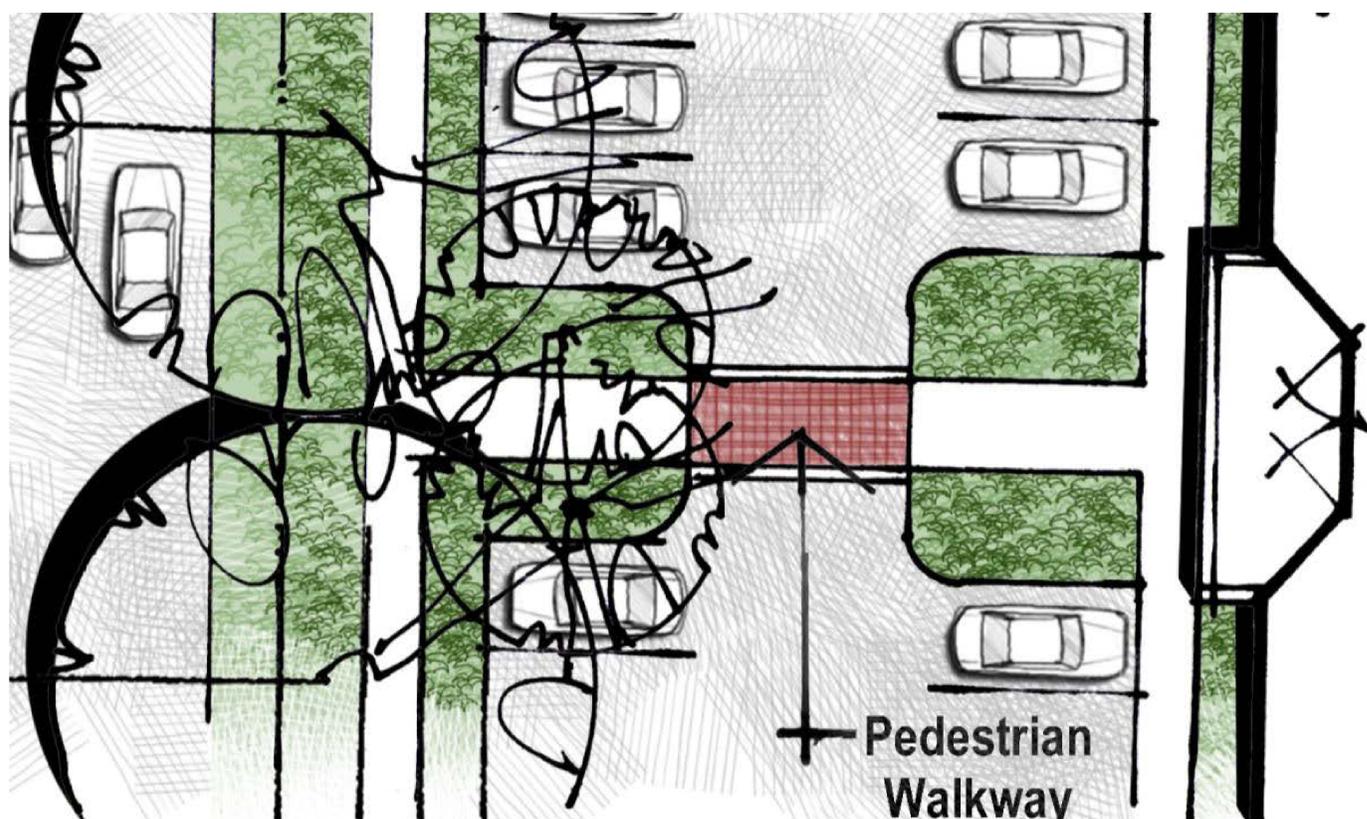
Pedestrian Facilities & Driveways

PEDESTRIAN FACILITIES

- 8' sidewalks along public streets/roads
- 8' walkways from the public sidewalk or right-of-way to the principal customer entrance
- Walkway to the common property boundary of multi-family and/or nonresidential parcels
- 8' sidewalks along any facade featuring a customer entrance, and along any facade abutting public parking areas (additional for outdoor seating areas)
- Walkways and crosswalks in parking areas shall be distinguished from asphalt driving surfaces
- Maintained by property owner unless dedicated

DRIVEWAYS

- Subject to the standards and permitting processes of SCDOT
- No more than one vehicular access point (driveway)
- No driveway within 400 feet of an intersection of any other public road on the Corridor.
- Driveways shall be a minimum of 400 feet apart (CL to CL), and shall align with opposing driveways
- Shared drives, parallel access roads (COD-N only)
- Cross access easement may be required
- Alts approved by SCDOT
- Adjacent parcels in common ownership fronting on the Corridor considered as one parcel.



Southern Bypass Corridor Overlay District

Parking

OFF-STREET

- All off-street parking shall be provided in accordance with the off-street parking requirements set forth in Article I, Section 7, subsection I of the Ordinance.
- Landscaping in off-street parking lots shall meet the requirements of Article IV.
- Landscaped buffers separating off-street parking lots and one-family residential uses and lots.
- Shared parking within 1,200 feet of each respective use (subject to parking analysis and written agreement)
- Driveway connections between adjacent nonresidential parcels.
- Off-street parking shall be located to the side or rear of any structure nearest to a Town-maintained road or private street.
- Where parking is located between a structure and a Town-maintained road or private street, it shall be limited to one bay of parking (i.e., two rows of parking spaces with one shared drive aisle between the rows of spaces).
- All off-street parking in multi-family and nonresidential shall be screened from public view.
- All commercial off-street parking shall be subject to the requirements of the Appearance Review Ordinance as well as the Appearance Review Process.

ON-STREET

- No on-street parking shall be located on the Bypass.
- In COD-N, a min. of 50% off-street on site, provided:
- A key map is provided that delineates the location of allocated on-street spaces for a designated parcel or use.
- The on-street parking must be located within 1,200 feet of the primary entrance.
- On-street parallel parking spaces shall be 7' x 20' measured from the face of curb (or edge of pvmt.)
- On-street diagonal parking with a 60-degree angle or less shall have a minimum travel lane width of 11 feet.



Southern Bypass Corridor Overlay District

Freestanding Signs

Freestanding identification signs for nonresidential and multi-family uses are permitted along the Corridor in accordance with the following:

- No parcel with less than 50 feet of frontage on a Corridor shall be permitted to have a freestanding sign.
- Parcels with 50 to 200 feet of frontage on the Corridor may be permitted to have one free standing sign.

- Maximum height: 4 feet in height
- Maximum sign face area: 0.5 sq. ft. per 2 linear feet of frontage , up to 30 sq. ft. (total)
- Minimum setback from ROW: 5 feet

- Parcels with more than 200 feet of frontage on the Corridor may be permitted to have up to two free standing signs.

- Maximum height: 7 feet in height
- Maximum sign face area (total): 50 sq. ft.
- Minimum setback from ROW: 5 feet

- In addition, nonresidential multi-tenant developments on parcels with more than 200 feet of frontage are permitted the following, provided such sign is consistent with the unified sign plan approved by the Town for the development.

- Maximum height: 30 feet in height
- Maximum sign face area (total):
 - Floor area <50,000 sq. ft. 100 sq. ft.
 - Floor area of 50,000-100,000 sq. ft. 125 sq. ft.
 - Floor area >100,000 sq. ft. 150 sq. ft.
- Minimum setback from ROW: 10 feet

Internal lighting of signs, neon, LED, and flashing signs shall not be permitted along the Corridor. Building flood-lighting shall not be permitted, except in COD-N.

The provisions of this subsection shall not apply to temporary signs.

All other sign standards pertaining to freestanding signs per section shall apply along other street frontages.



Southern Bypass Corridor Overlay District

Streetscape

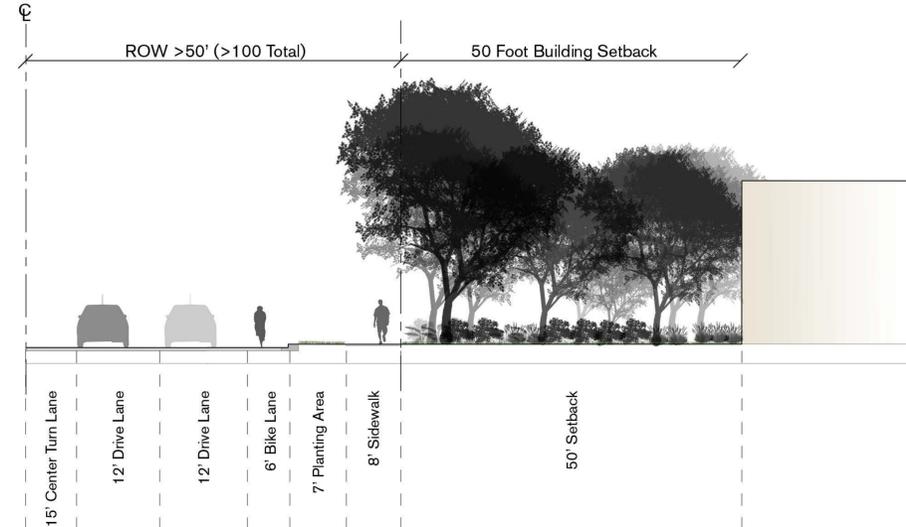
COD

Trees within 50 feet of the right-of-way shall be protected in accordance with Section 3, Article IV, Landscaping Standards.

- Natural. Where there is no existing vegetation to be preserved:
- If the 50-foot minimum setback is utilized, plant per a 50-foot landscaped buffer.
- If the 100-foot minimum setback is utilized, plant per a 20-foot landscaped buffer.

Street trees shall not be required. Canopy trees may be combined with understory trees, uniformly spaced or clustered. Canopy trees shall not be less than 40 feet on center.

COD-O Streetscape Scenario 1



COD-O Streetscape Scenario 2

