The background of the slide features a photograph of a road curving to the right, bordered by a grassy area and a line of trees in the distance. A semi-transparent green rectangular overlay covers the middle portion of the image, serving as a background for the main title text.

Southern Bypass Corridor Overlay District

**Public Meeting
October 2, 2013**

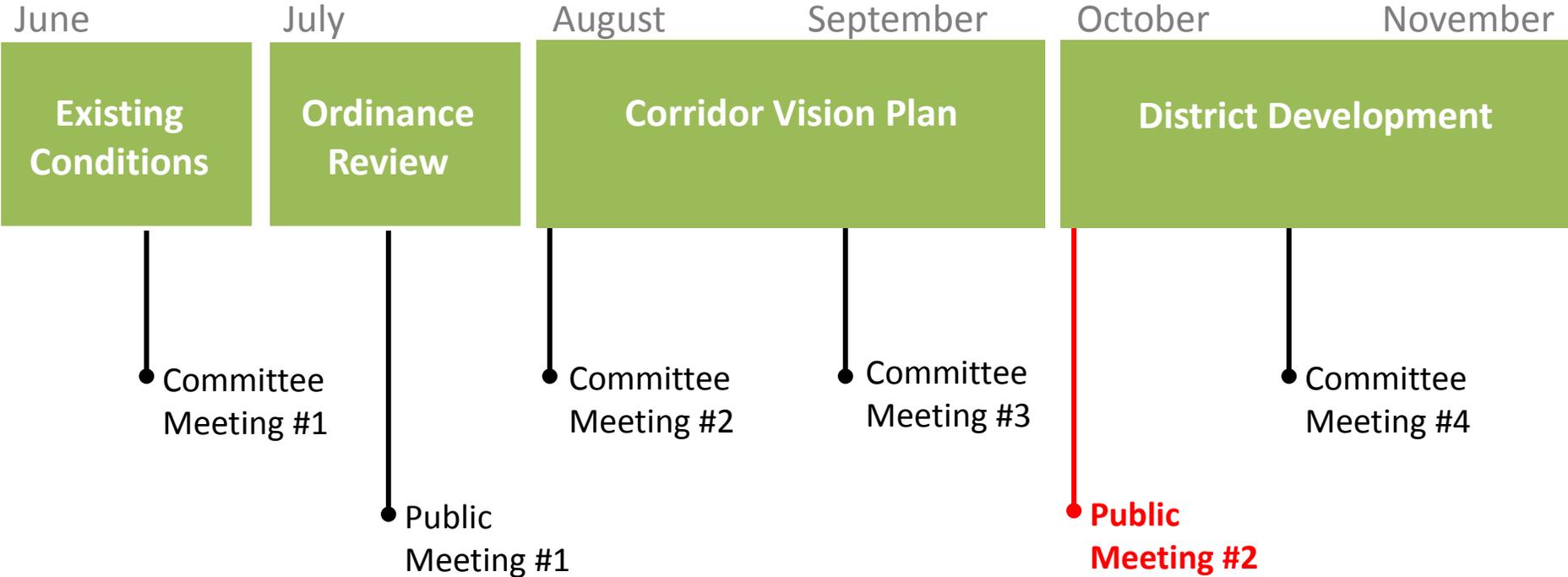
Meeting Agenda

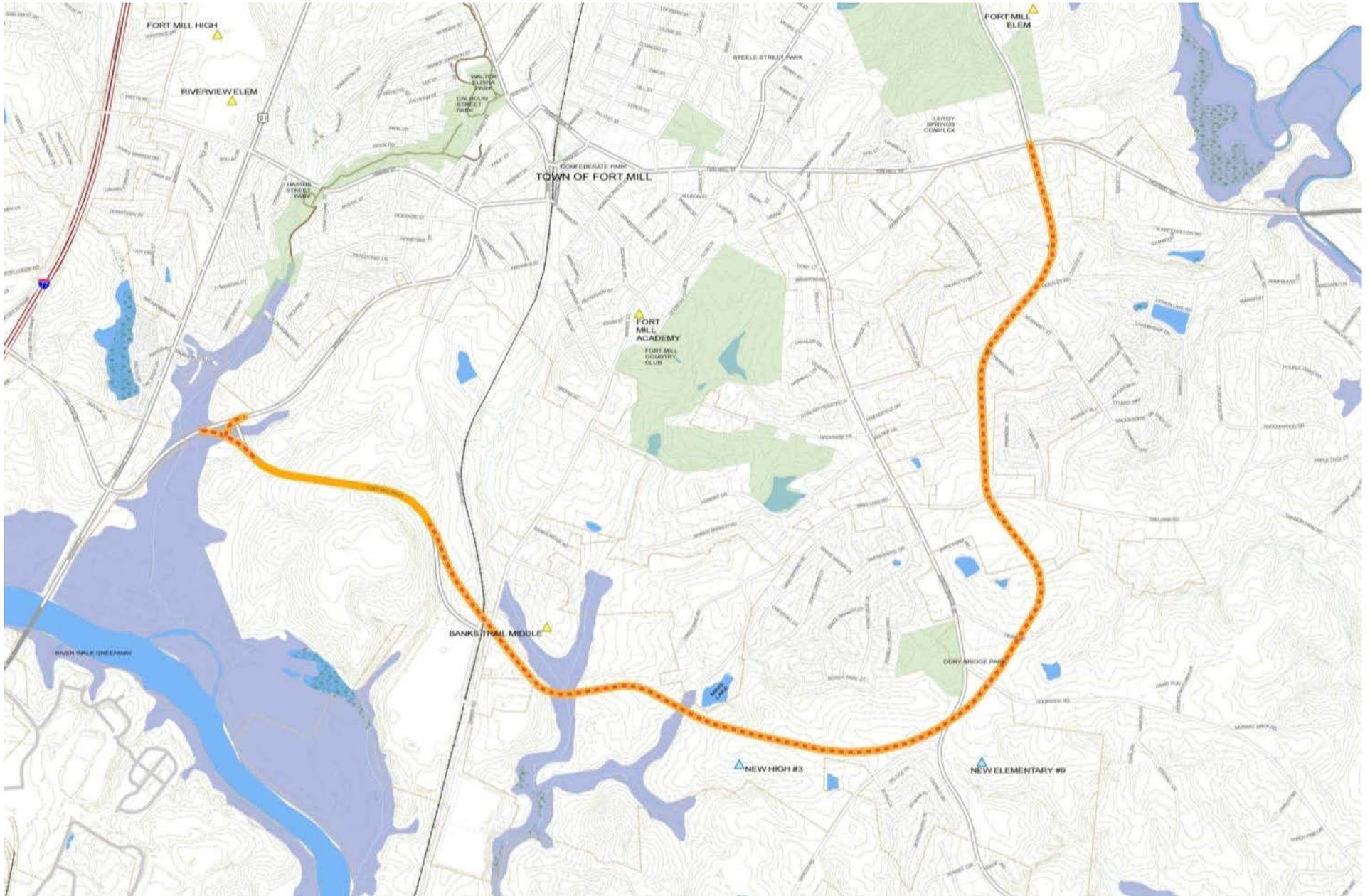
- Project Update
- Presentation of Draft District
- Q & A

The background image shows a paved path curving through a park. On the right side of the path, there is a white wooden fence. In the foreground on the left, there are two signs: a 'PLEASE SLOW DOWN' sign with a pedestrian icon and a 'SPEED LIMIT 15' sign. The scene is filled with trees and greenery, and a semi-transparent green grid overlay is present in the center.

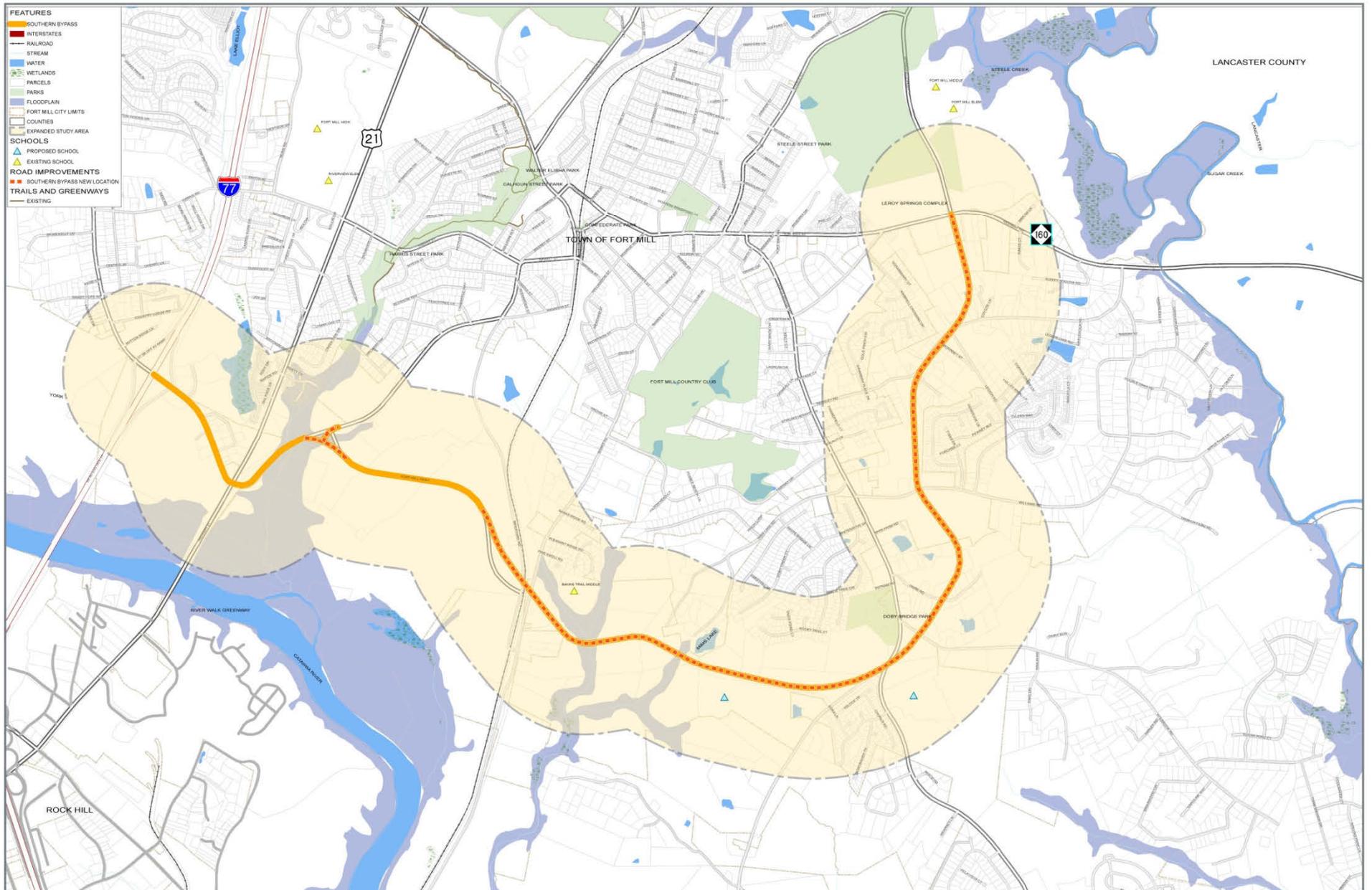
Project Update

Corridor Overlay District Scope & Schedule

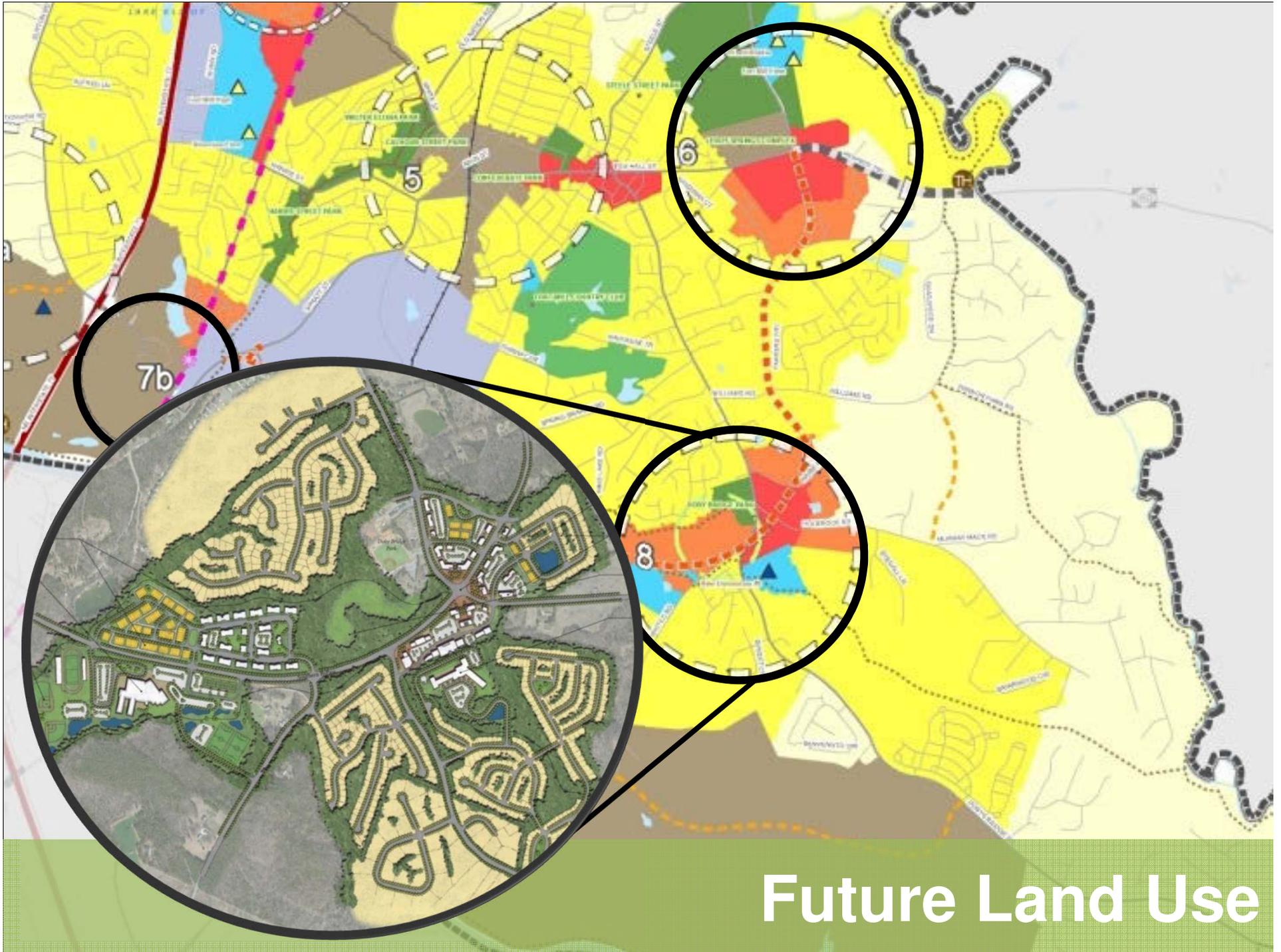




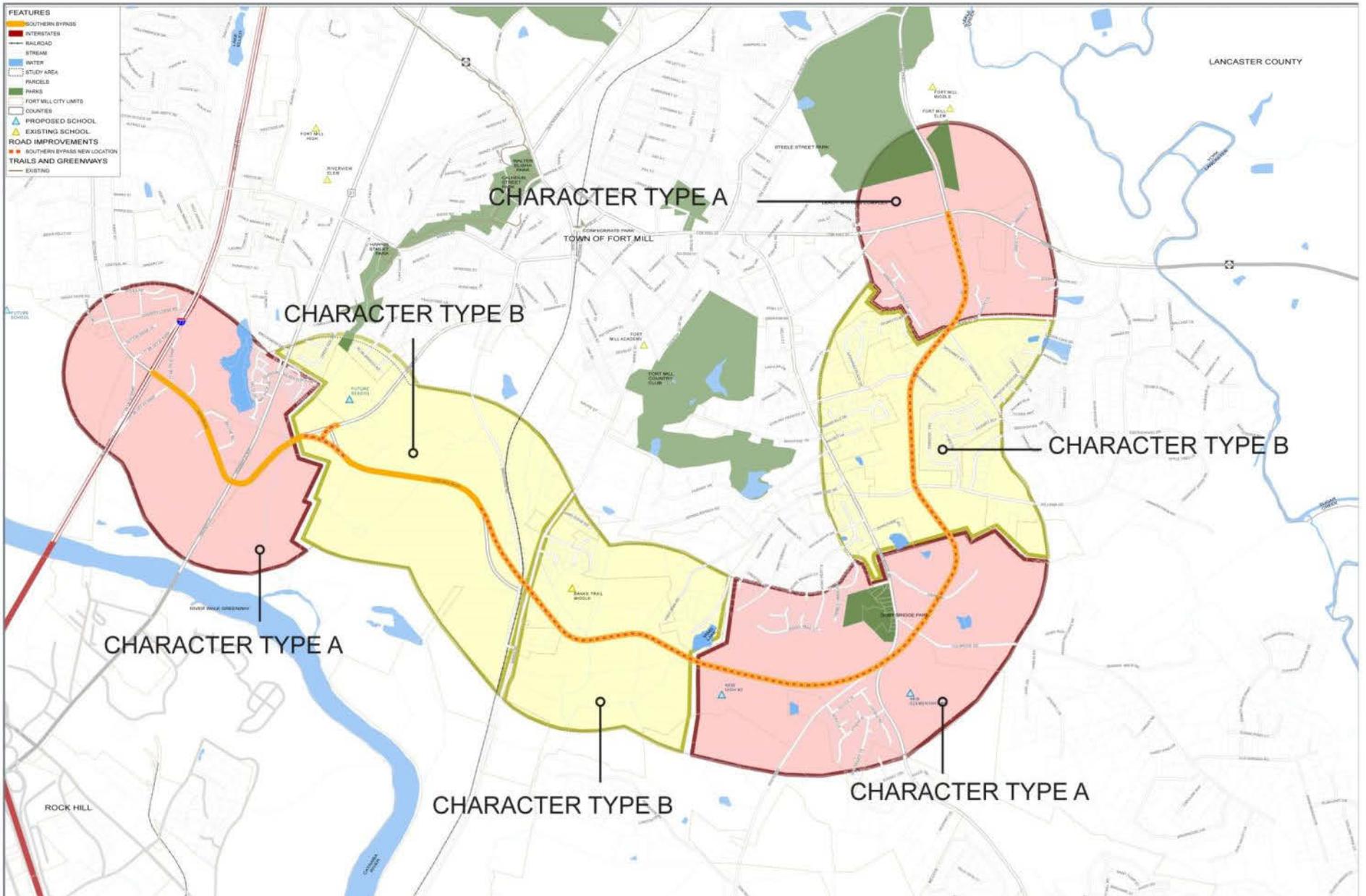
Bypass Project



Expanded Study Area



Future Land Use

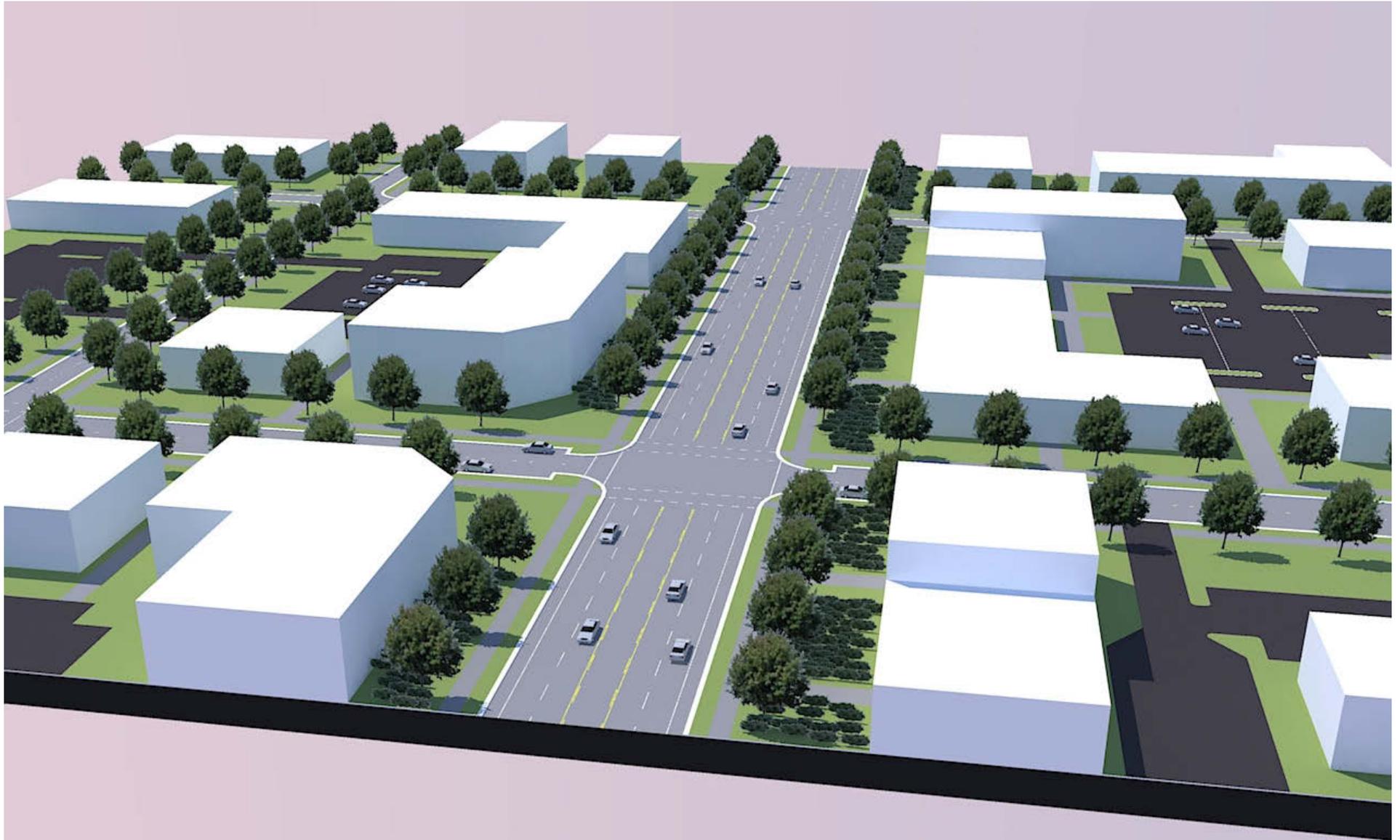


Simplified Character Areas

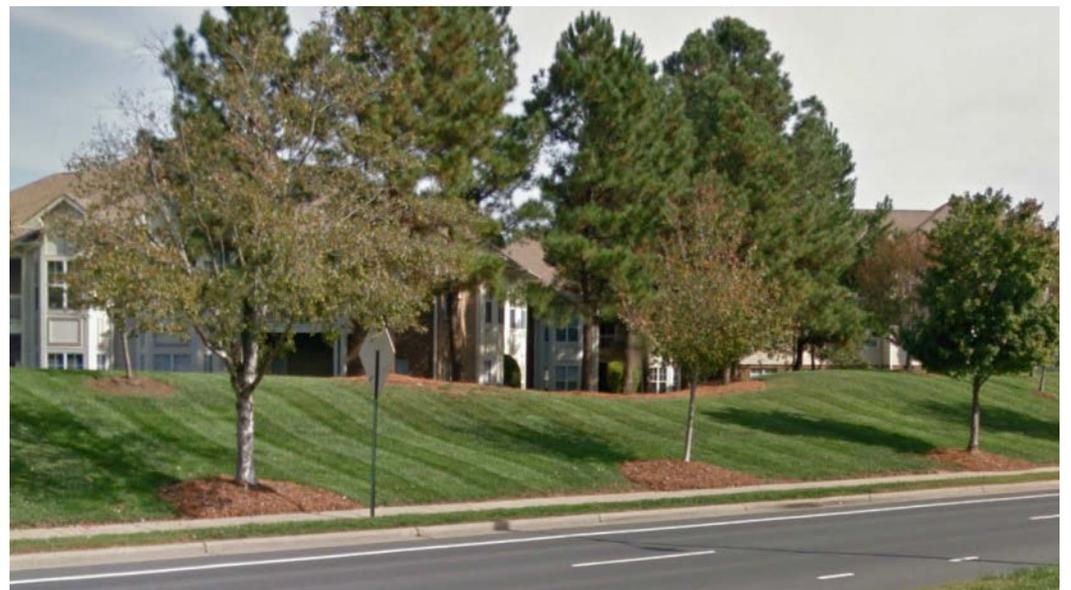
TOWN OF
FORT MILL

Initial Concepts

DOBY BRIDGE
PARK



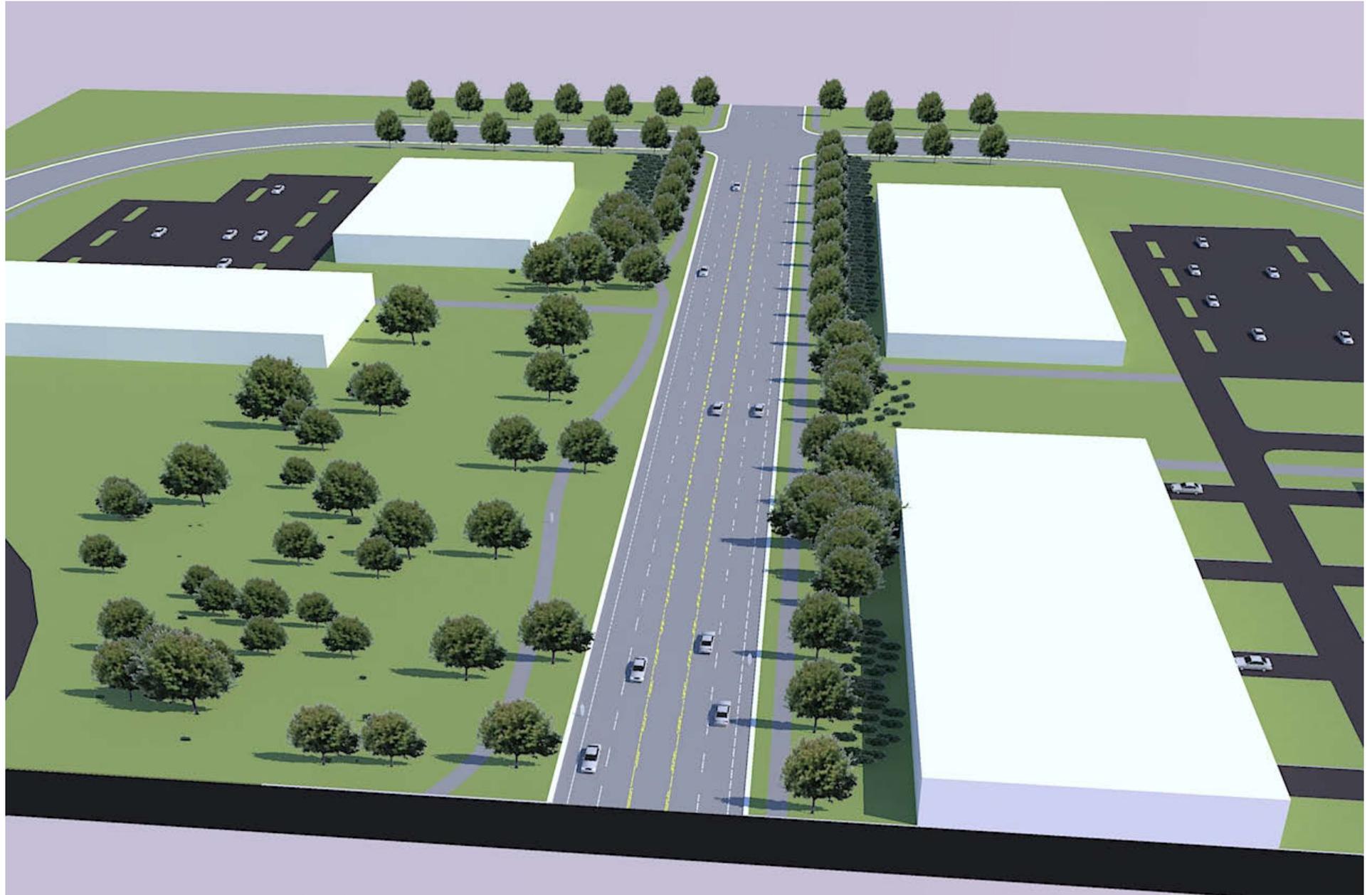
Design Concept – Node



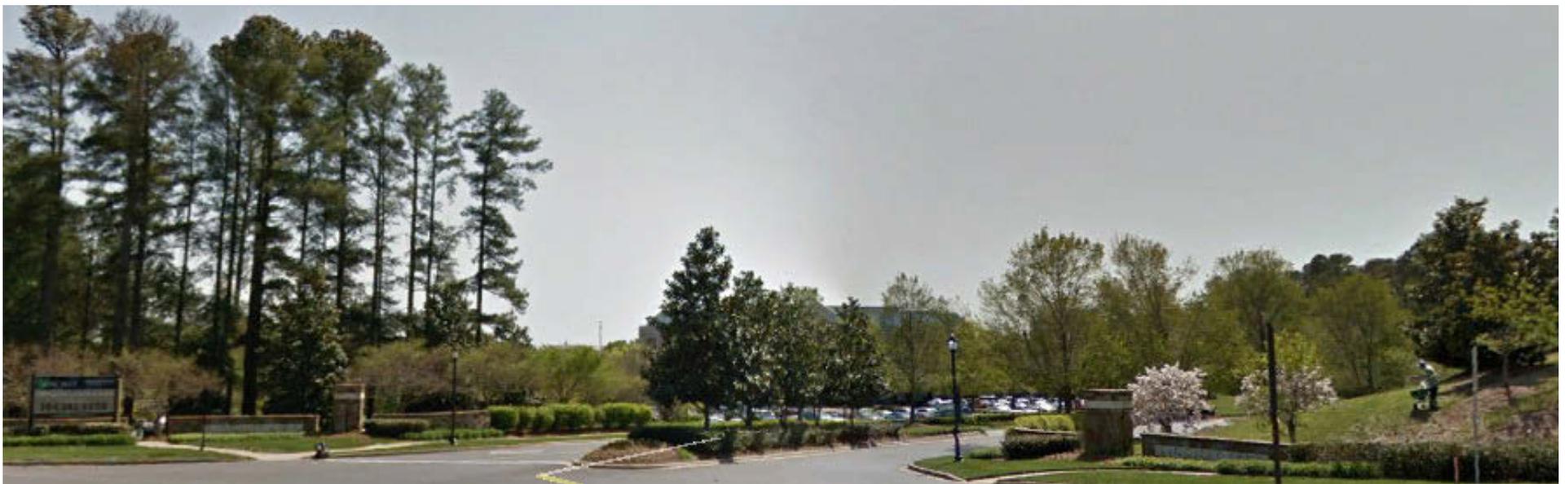
Design Concept – Node



Design Concept – Node



Design Concept – Other



Design Concept – Other



Design Concept – Other

The background image shows a street scene with a road, a sidewalk, and a row of trees in the distance. A semi-transparent green banner with a grid pattern is overlaid across the middle of the image, containing the title text.

Draft Corridor Overlay District (COD)

Purpose

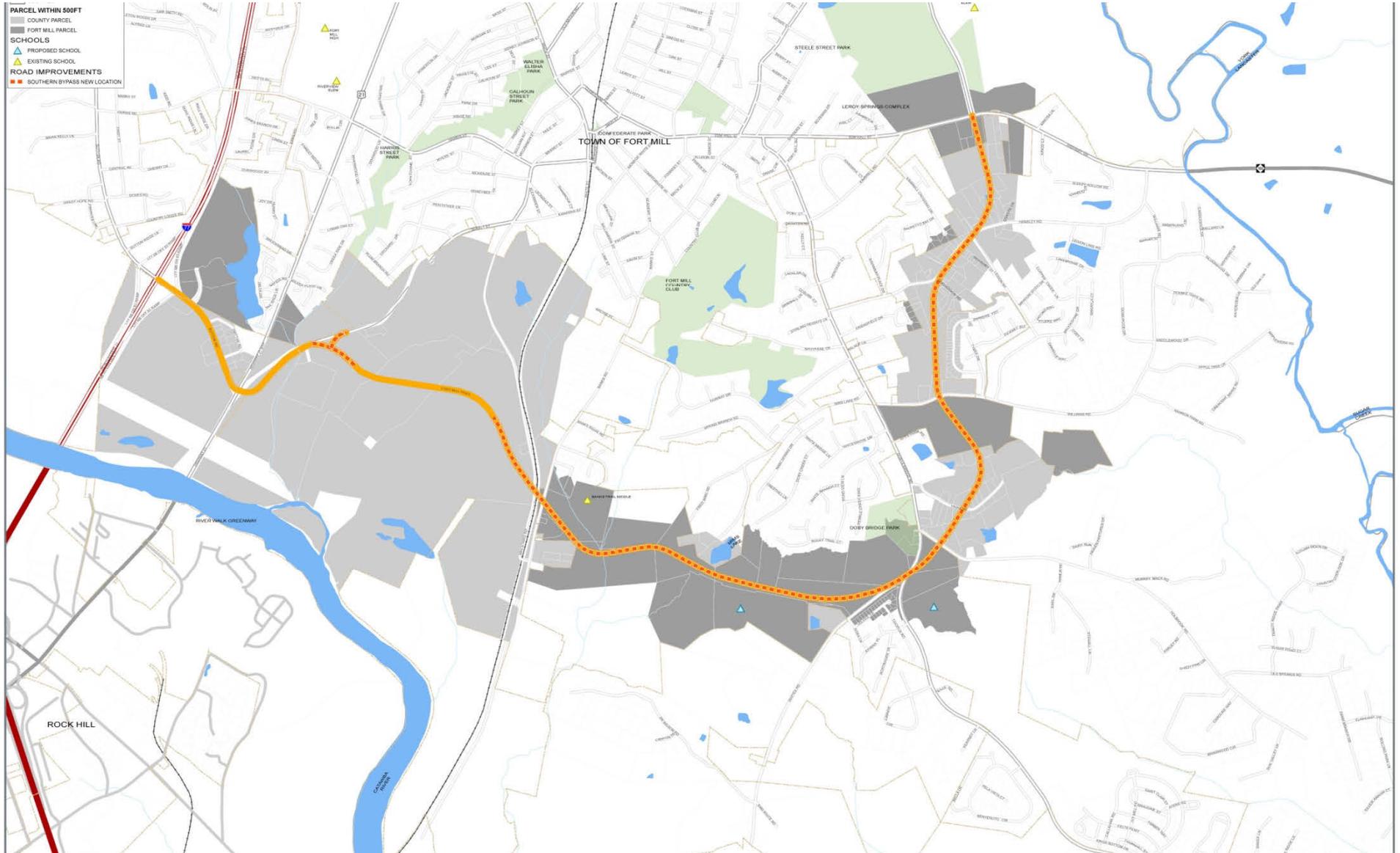
The Corridor Overlay District (COD) is established for the purpose of maintaining a safe, efficient, functional and attractive roadway corridor for the Fort Mill Southern Bypass (Bypass). It is recognized that, in areas of high visibility, the protection of features that contribute to the character of the area and enhancements to development quality promote economic development and stability in the entire community.

Applicability

Parcels in the COD-U and the COD-R shall be as designated on the official Zoning Map. Such parcels shall be adjacent to the Bypass right-of-way (Corridor) and **wholly or in part within 500 feet** of the outer edge of the right-of-way of the Corridor. Where part of a parcel is within 500 of the right-of-way of the Corridor, the entire parcel shall be subject to these regulations, unless specifically excluded below.

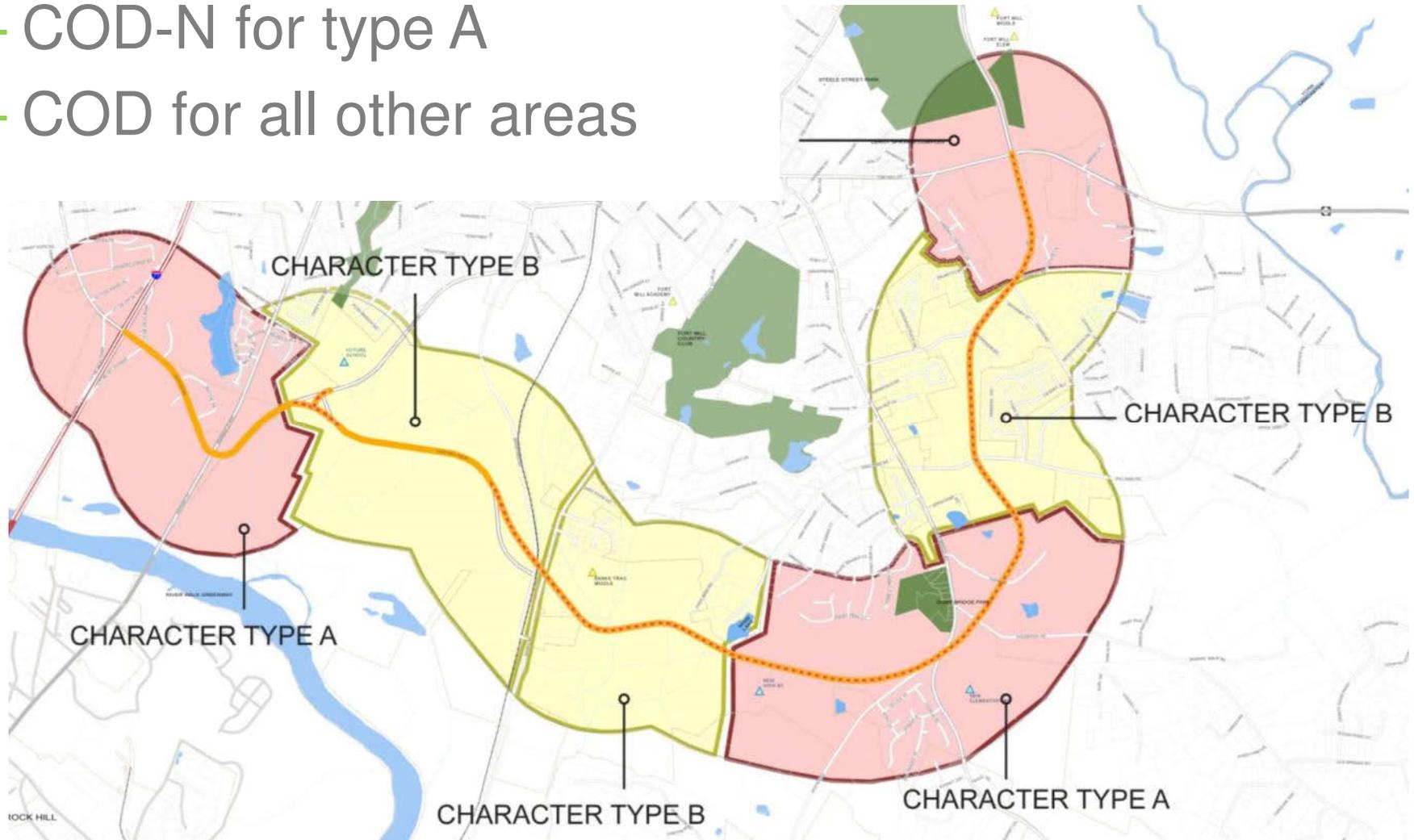
The standards established in this section shall be applied **to single-family subdivisions (ltd), multifamily residential and all nonresidential development** which lies within the area delineated on the Zoning Map.

Applicability



Applicability – COD and COD-N

- COD-N for type A
- COD for all other areas



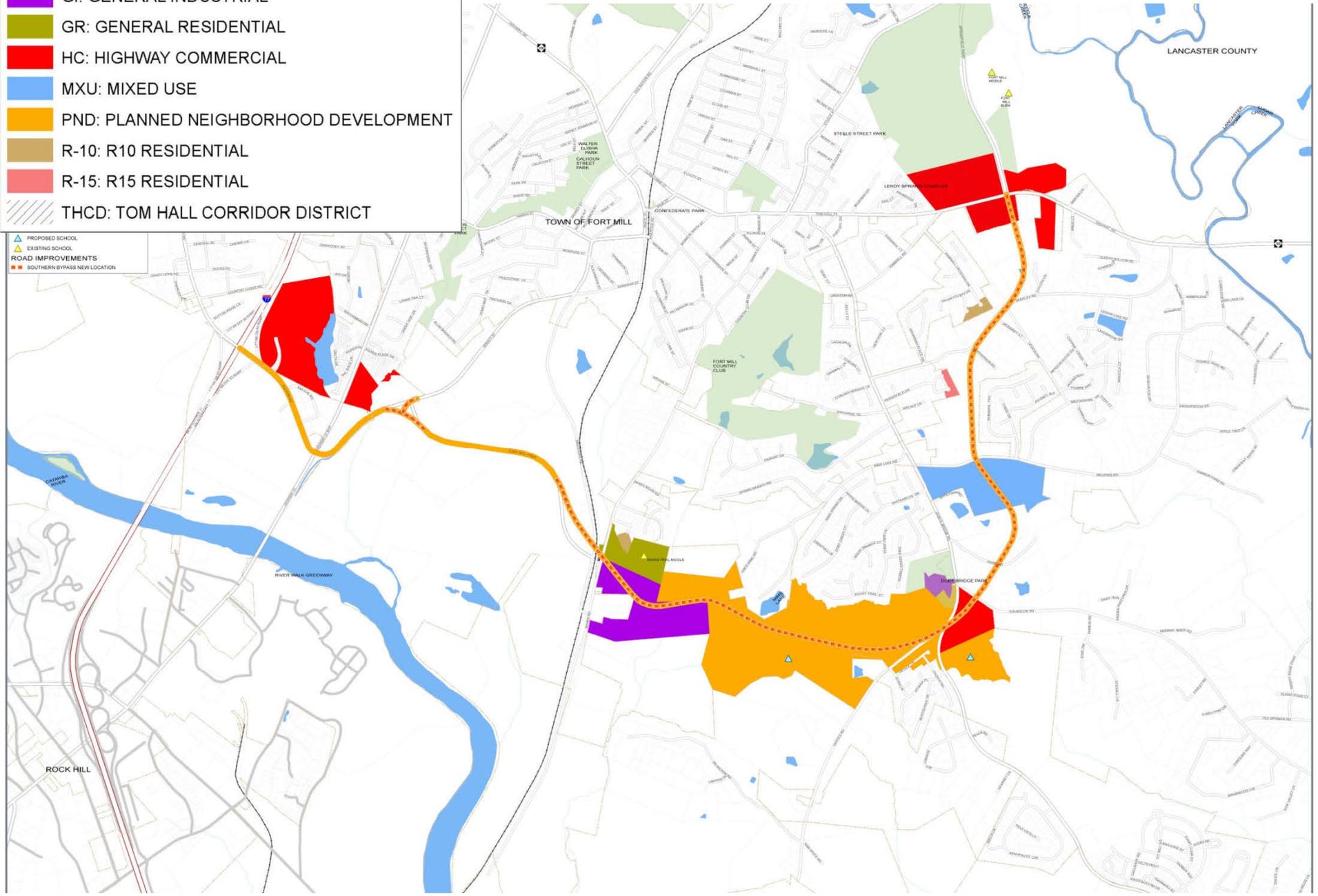
Permitted Uses

All permitted, special and conditional uses of the **underlying zoning district** are allowed subject to the specific requirements and procedures for each use classification, except...

ZONING

- GI: GENERAL INDUSTRIAL
- GR: GENERAL RESIDENTIAL
- HC: HIGHWAY COMMERCIAL
- MXU: MIXED USE
- PND: PLANNED NEIGHBORHOOD DEVELOPMENT
- R-10: R10 RESIDENTIAL
- R-15: R15 RESIDENTIAL
- THCD: TOM HALL CORRIDOR DISTRICT

- PROPOSED SCHOOL
- EXISTING SCHOOL
- ROAD IMPROVEMENTS
- SOUTHERN BYPASS NEW LOCATION



except... **Prohibited Uses**

- Automobile rental and sales.
- Automotive wrecker service.
- Casino or gambling establishment.
- Check cashing establishments, title loan lenders, deferred presentment lenders, pawnshops, loan brokers, and small loan companies.

except... **Prohibited Uses (cont'd)**

- Communications towers.

Where such towers must be permitted per the Telecommunications Act of 1996 and it has been demonstrated that no existing towers or structures (such as rooftops, water towers, etc.) can accommodate such equipment, the towers shall not exceed 100 feet in height. To the extent practicable, they shall be roof-mounted, not freestanding, structures.

- Industrial or heavy manufacturing uses (prohibited in COD-N only).
- Junk or salvage yards.
- Mobile homes.
- Sexually-oriented businesses.

Dimensional Requirements

The dimensional requirements in the COD-U and COD-R districts shall be the same as the **underlying zoning district**, except as follows...

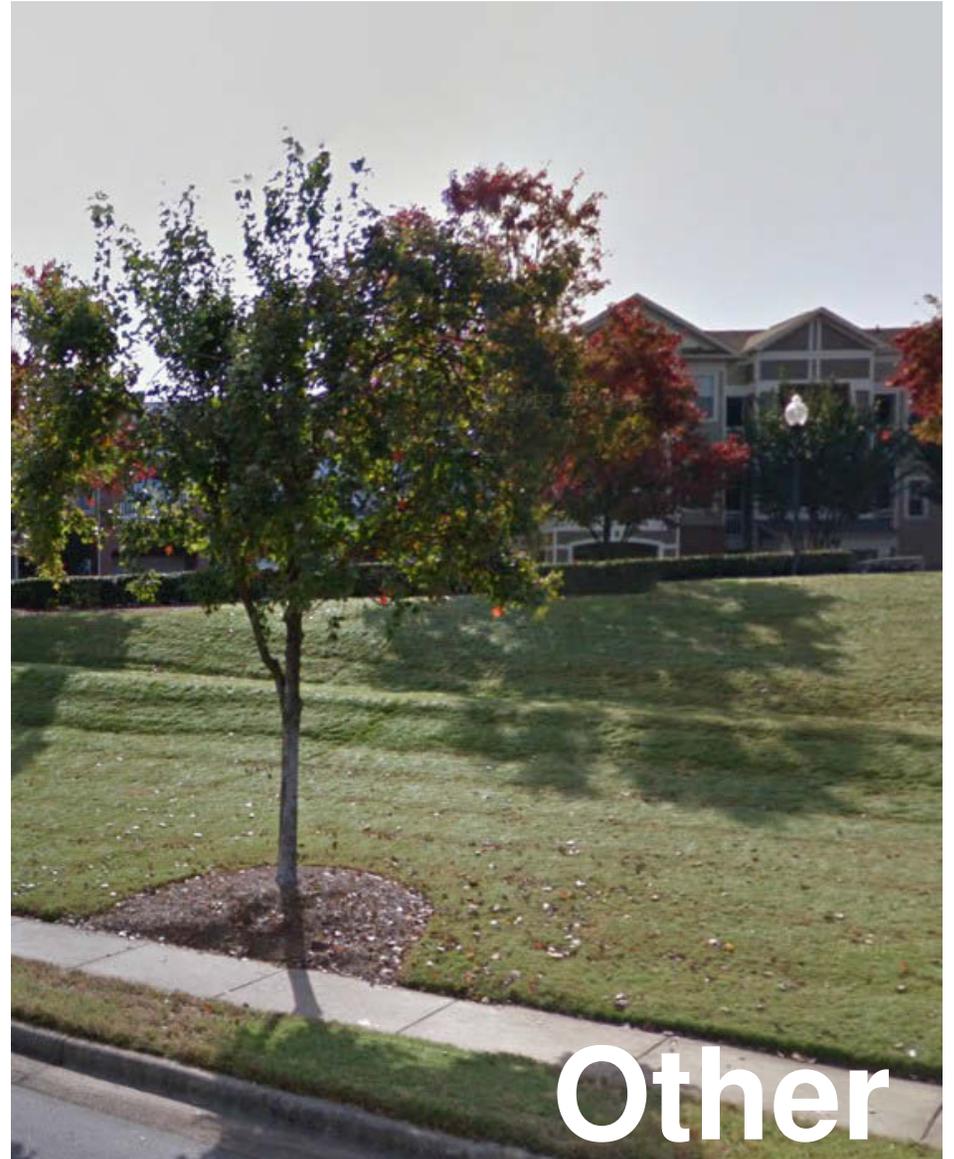
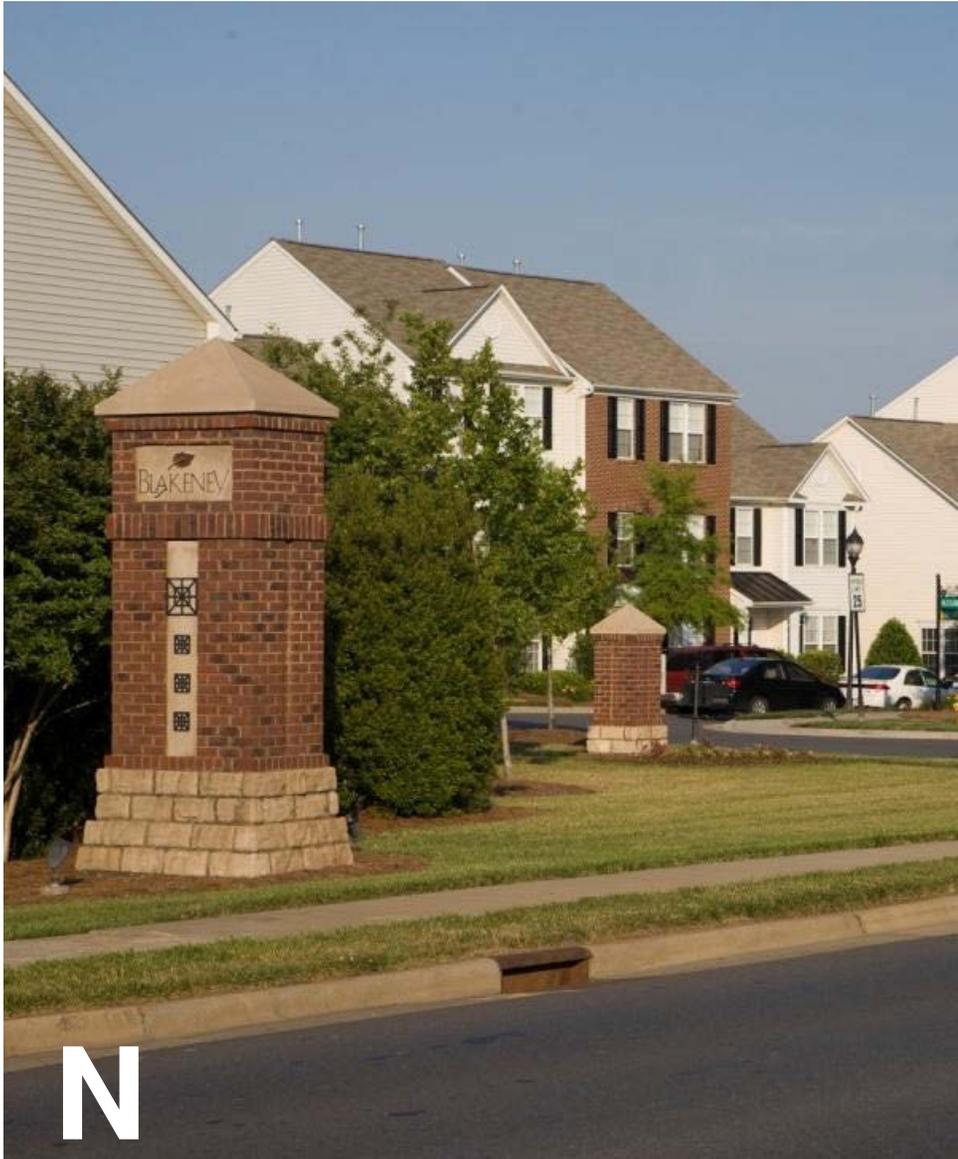
Dimensional Requirements

Setbacks

The building setbacks of the underlying zoning districts shall apply, except along the Corridor. Building setbacks, measured from the ROW, shall be as follows:

	<u>Minimum</u>	<u>Maximum</u>	<u>Avg</u>
COD-N	35'	None	45'
COD	50' or 100' (landscaped buffer req. vary)	None	NA

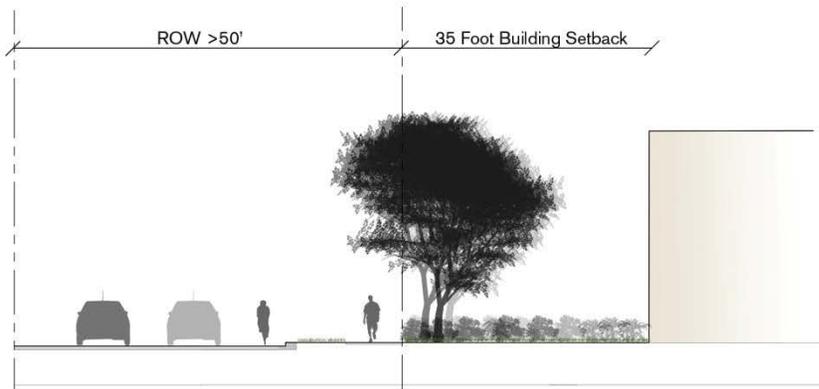
Dimensional Requirements – Setbacks



Dimensional Requirements – Setbacks



COD-N Typical Building Setback

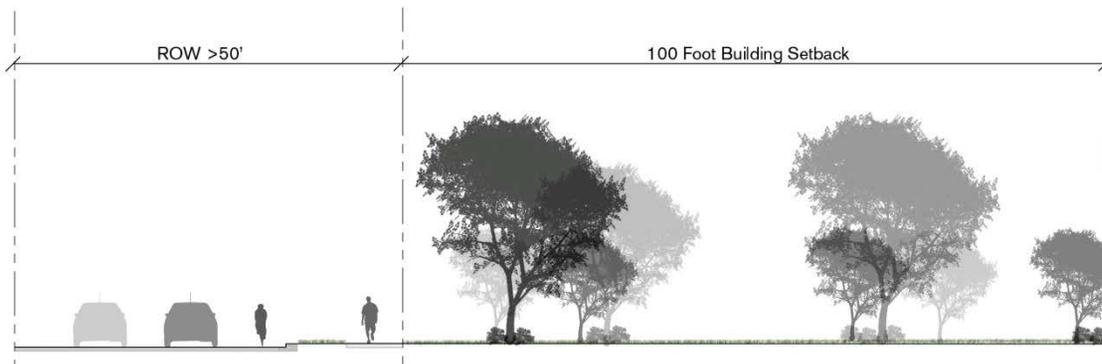


Setbacks Options

COD-O 50 Foot Building Setback



COD-O 100 Foot Building Setback



Dimensional Requirements – Setbacks

The minimum setback in COD shall be reduced in cases where the setback area exceeds 40% of the acreage of a parcel already in existence. The width of the setback area shall be reduced to the extent necessary (up to a 15-foot reduction) in order that the buffer shall not exceed 40% of the parcel. No setback shall be less than 35 feet.

Dimensional Requirements – Setbacks

There shall be no development allowed in the setback, except as follows:

- Drainage features;
- Driveways;
- Landscaping;
- Lighting;
- Parks and park-like facilities (not including ball fields);
- Public utilities (limited to lines and other equipment);
- Retaining wall(s);
- Sidewalks, pedestrian ways, or bikeways;
- Signs;
- Streetscape elements.

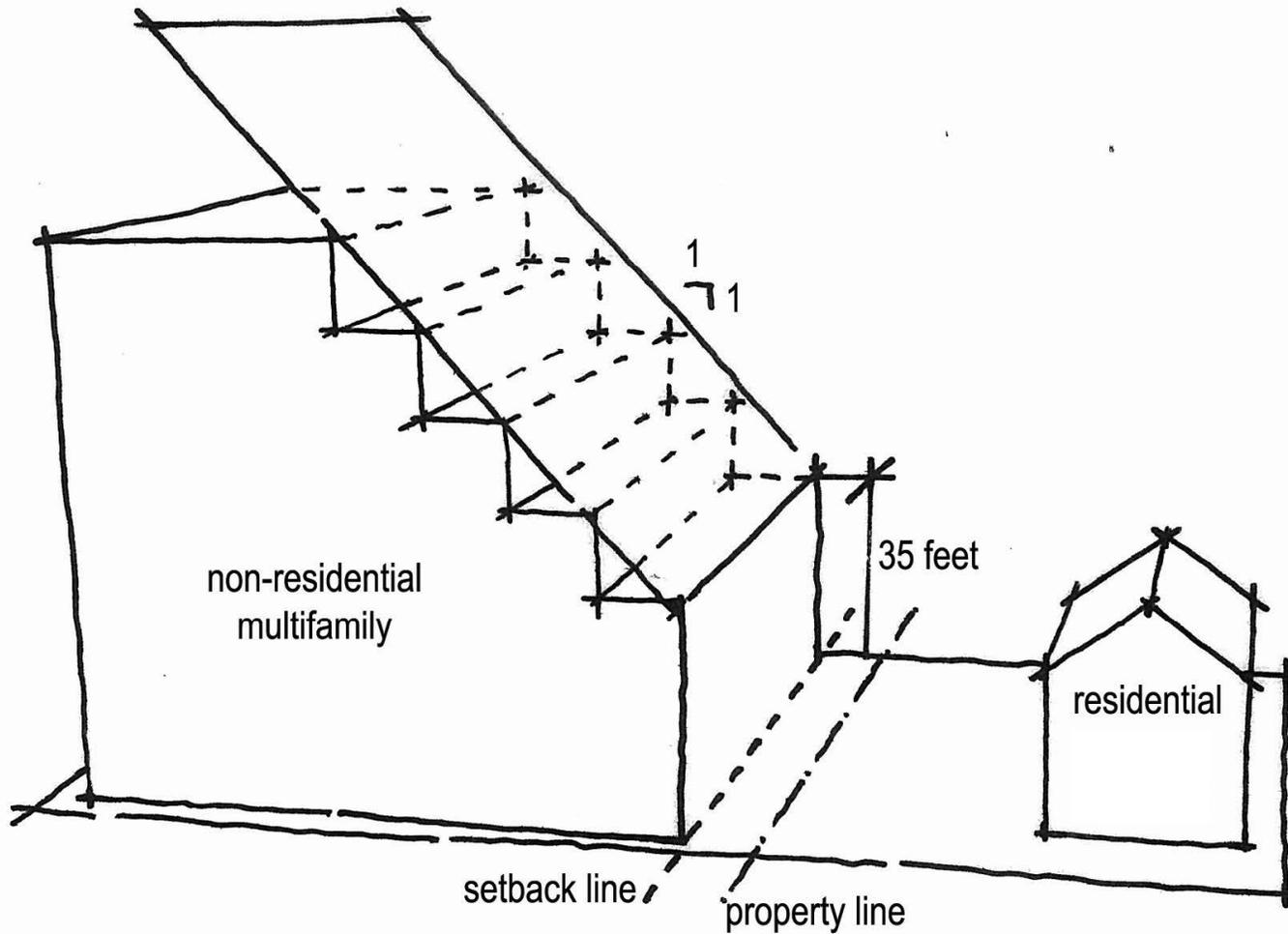
Dimensional Requirements - Height

Building Height

	<u>Minimum</u>	<u>Maximum</u>
COD-U	24'	45'
COD-R	NA	Underlying Zoning

Height may be above the maximum height indicated, provided all portions of the structure exceeding the height limit indicated shall be stepped back an additional one foot from the adjoining property line for each additional foot in excess.

Dimensional Requirements - Height



Building Design – Orientation

Loading areas of buildings shall not be visible from the Corridor. Such loading areas shall be oriented away from view from the Corridor or shall be screened.

Building Design – Orientation

The rear facades of buildings in COD shall not be visible from the Corridor. Such facades shall be oriented away from view from the Corridor or shall be screened.

In COD-N, articulation of all non-residential facades, including rear, is required. Therefore, those rear facades do not have to be screened.

Building Design – Orientation | COD-N

Buildings shall be oriented toward the public street(s).

- Pedestrian access from the street is encouraged for all multi-family residential and nonresidential uses. Therefore, primary entrances shall be visible and accessible from the public street, where feasible.
- Where parking is provided at the rear of the building, the primary entrance may be located to provide access from such parking. Two primary entrances, one from the street and one from the rear parking area, are permitted.

Building Design – Architecture

Materials:

- Rock, stone, brick, stucco, concrete, wood and Hardiplank.
- No mirrored glass with a reflectance greater than 20 percent shall be permitted.
- Corrugated metal shall not be used on any façade.

In COD-N, any façade facing a street shall be articulated with architectural features and treatments to enhance the quality of pedestrian environment of the public street, particularly in the absence of a primary entrance.

Buffers

Landscaped buffers shall meet the following:

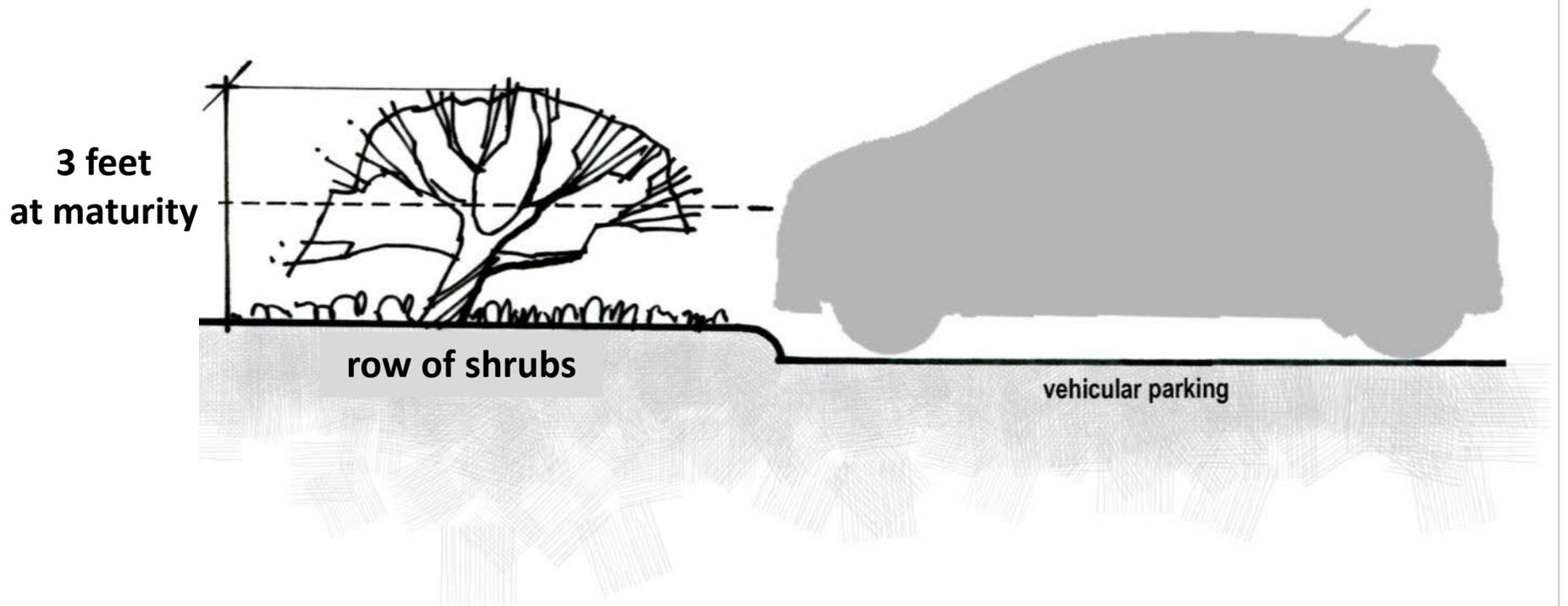
- 20 to 50 feet in width
- Natural, undisturbed
- Trees shall be protected in accordance with Section 3, Article IV, Landscaping Standards.
- Plant for every 100' without vegetation or if disturbed :

• <5 acres	5-10 acres	>10 acres
• 20 feet	35 feet	50 feet
• 3 canopy trees	5 canopy trees	5 canopy trees
• 6 understory	10 understory	10 understory
• 9 shrubs	15 shrubs	20 shrubs

Screening

- Multifamily and nonresidential development against SF residential:
 - Landscaped buffer
 - Evergreen trees and shrubs
 - required buffers shall be located entirely on the parcel of the developing multifamily and non-residential use
- Loading areas and some rear facades – landscaped buffer, fence, wall, or combination
- Parking areas of multifamily and nonresidential: shrubs five feet on center, at least three feet in height at maturity

Screening: Parking – Off-Street



Streetscape | COD-N

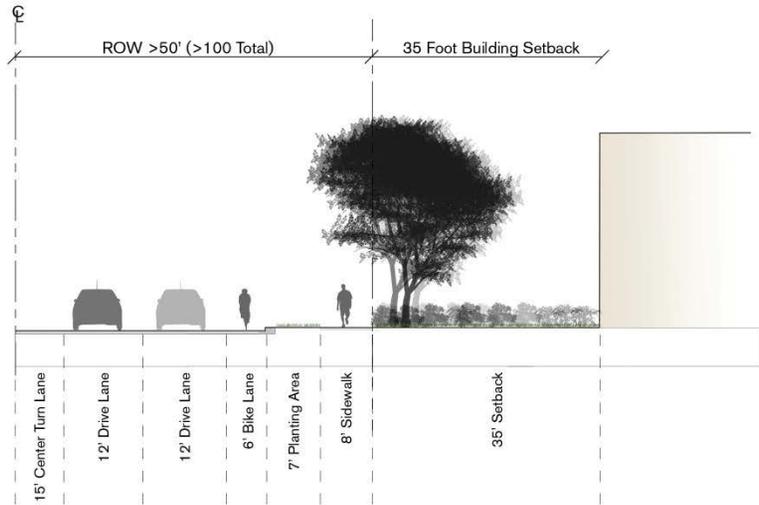
Street trees shall consist of canopy trees planted within the streetscape zone (the first 15 feet of the setback) at rate of one tree per 50 linear feet along all Corridor frontages. Tree spacing shall be not more than 60 feet and not less than 40 feet on center. At planting, street trees shall be a minimum of 2" caliper or 8 feet in height.

Streetscape | COD-N

All new development or expansions of existing development resulting in a minimum ten percent increase in building floor area or lot area shall provide landscaping in setback:

- Trees - for every 2,500 square feet of planting area
 - a minimum of two trees shall be planted
 - at least 50% canopy trees
 - street trees shall be counted
- Shrubs - for every 2,500 square feet of planting area
 - a minimum of ten shrubs shall be planted
 - at least 50% of the shrubs planted shall be evergreen
 - at least 3' at maturity

COD-N Typical Streetscape



Streetscape | COD-N



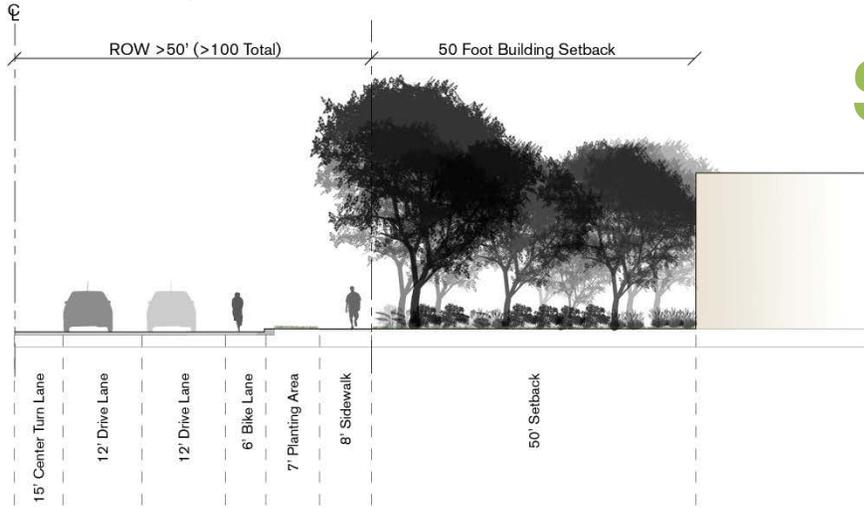
Streetscape | Other COD

Trees within 50 feet of the right-of-way shall be protected in accordance with Section 3, Article IV, Landscaping Standards.

Natural. Where there is no existing vegetation to be preserved:

- If the **50-foot** minimum setback is utilized, plant per a **50-foot landscaped buffer**.
- If the **100-foot** minimum setback is utilized, plant per a **20-foot landscaped buffer**.
- Street trees shall not be required. Canopy trees may be combined with understory trees, uniformly spaced or clustered. Canopy trees shall not be less than 40 feet on center.

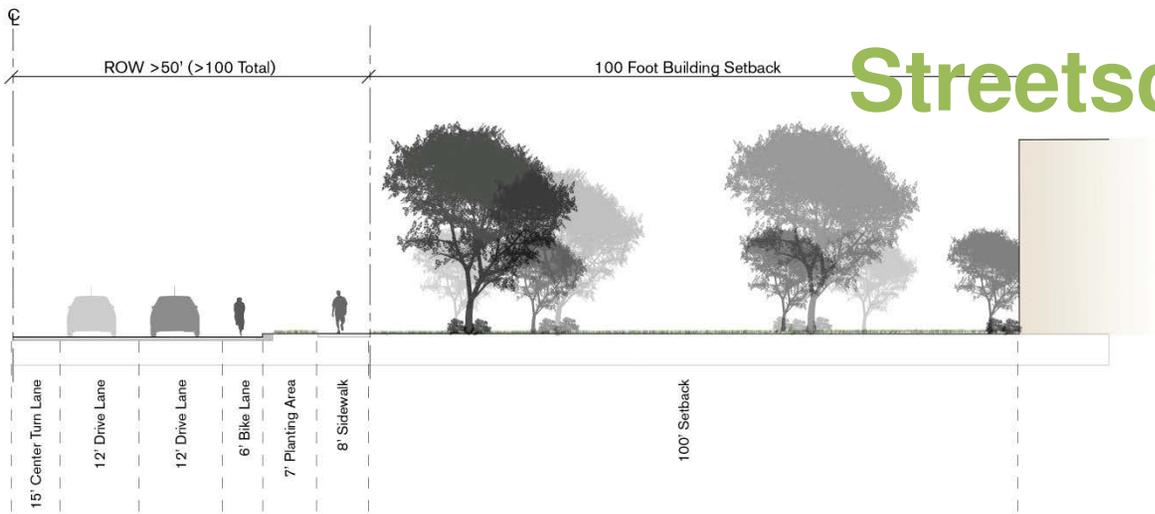
COD-O Streetscape Scenario 1



Streetscape | Other COD



COD-O Streetscape Scenario 2



Streetscape | Other COD



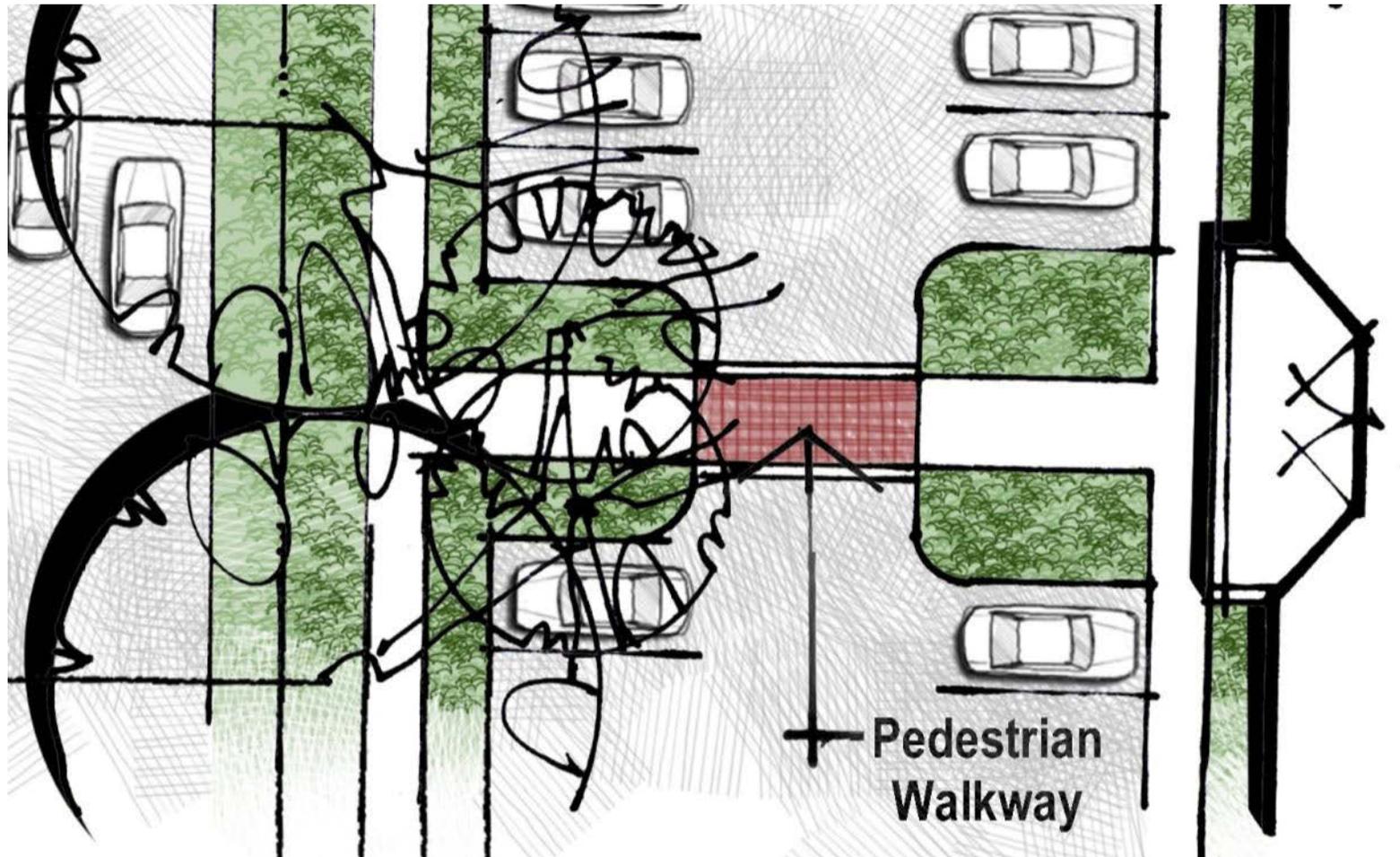
Lighting (COD-N only)

Lighting shall be installed in the streetscape zone along the Corridor in COD-N in accordance with the fixture spacing, height, color and type requirements specified in the lighting plan/policy (or streetscape plan that includes a lighting plan) adopted by the Town for that COD-N segment of the Corridor, if such plan/policy exists. All other lighting standards of Section 6 of Article IV shall apply.

Pedestrian Facilities

- 8' sidewalks along public streets/roads
- 8' walkways from the public sidewalk or right-of-way to the principal customer entrance
- Walkway to the common property boundary of multi-family and/or nonresidential parcels
- 8' sidewalks along any facade featuring a customer entrance, and along any facade abutting public parking areas (additional for outdoor seating areas)
- Walkways and crosswalks in parking areas shall be distinguished from asphalt driving surfaces
- Maintained by property owner unless dedicated

Pedestrian Facilities



Driveways on Corridor

- Subject to the standards and permitting processes of SCDOT
- **No more than one** vehicular access point (driveway)
- No driveway within **400 feet** of an intersection of any other public road on the Corridor.
- Driveways shall be a minimum of **400 feet** apart (CL to CL), and shall align with opposing driveways
- **Shared** drives, **parallel access** roads (COD-N only)
- **Cross access easement** may be required
- Alts approved by SCDOT
- Adjacent parcels in common ownership fronting on the Corridor considered as one parcel.

Parking – Off-Street

- All off-street parking shall be provided in accordance with the off-street parking requirements set forth in Article I, Section 7, subsection I of the Ordinance.
- **Landscaping** in off-street parking lots shall meet the requirements of Article IV.
- Landscaped **buffers** separating off-street parking lots and single-family residential uses and lots.
- Shared parking **within 1,200 feet** of each respective use (subject to parking analysis and written agreement)
- Driveway connections between adjacent nonresidential parcels.

Parking – Off-Street

- Off-street parking shall be located **to the side or rear** of any structure nearest to a Town-maintained road or private street.
- Where parking is located between a structure and a Town-maintained road or private street, it shall be limited to **one bay** of parking (i.e., two rows of parking spaces with one shared drive aisle between the rows of spaces).
- All off-street parking in multi-family and nonresidential shall be **screened** from public view.
- All commercial off-street parking shall be subject to the requirements of the Appearance Review Ordinance as well as the Appearance Review Process.

Parking – Off-Street Bicycle Parking

5% of the minimum required off-street parking.

Located in close proximity to the primary entrance.

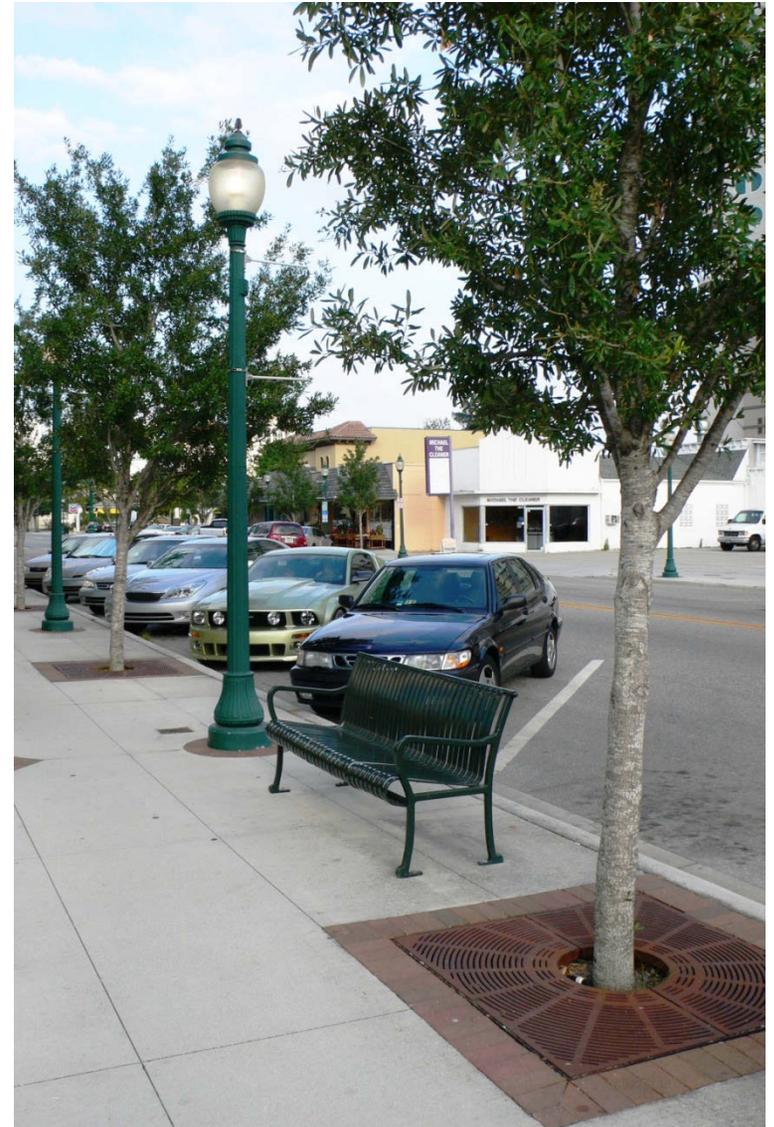
Designed to utilize bike racks installed on paved surfaces, and lighted for the safety, to discourage theft.

Not required if the entire development has a gross floor area of 10,000 square feet or less.



Parking – On-Street

- No on-street parking shall be located on the Bypass.
- In COD-N, a min. of 50% off-street on site, provided:
 - A key map is provided that delineates the location of allocated on-street spaces for a designated parcel or use.
 - The on-street parking must be located within 1,200 feet of the primary entrance.



Freestanding Signs

- Freestanding identification signs for nonresidential and multi-family uses are permitted along the Corridor in accordance with the following:
- No parcel with less than 50 feet of frontage on a Corridor shall be permitted to have a freestanding sign.

Freestanding Signs

- 50 to 200 feet of frontage: one free standing sign.
 - Maximum height: 4 feet
 - Maximum sign face area: 0.5 sq. ft. / 2 l.f. frontage , up to 30 sq. ft. (total)
 - Maximum setback from ROW: 5 feet
- >200 feet of frontage: up to two free standing signs.
 - Maximum height: 7 feet
 - Maximum sign face area (total): 50 sq. ft.
 - Minimum setback from ROW: 5 feet

Freestanding Signs

- Nonresidential multi-tenant developments, >200 feet of frontage and consistent with unified sign plan:
 - Maximum height: 30 feet
 - Maximum sign face area (total):
 - <50,000 sq. ft. 100 sq. ft.
 - 50,000-100,000 sq. ft. 125 sq. ft.
 - >100,000 sq. ft. 150 sq. ft.
 - Minimum setback from ROW: 10 feet

Freestanding Signs

Internal lighting of signs, neon, LED, and flashing signs shall not be permitted along the Corridor. Building floodlighting shall not be permitted, except in COD-N.

The provisions of this subsection shall not apply to temporary signs.

All other sign standards pertaining to freestanding signs shall apply along other street frontages.

Alternative Means of Compliance

Strict interpretation and application of the standards of this section may create particular hardships in areas of unusual topographic condition, or in retrofitting existing developed properties which are expanded. The Town Council may approve an alternate means of compliance (subject to findings).

In determining alternative means of compliance, the Town Council may consider various features such as width in relation to opacity of landscaping in setback, number of required parking spaces, and driveway locations and/or distances from nearby intersections.

Alternative Means of Compliance

...findings:

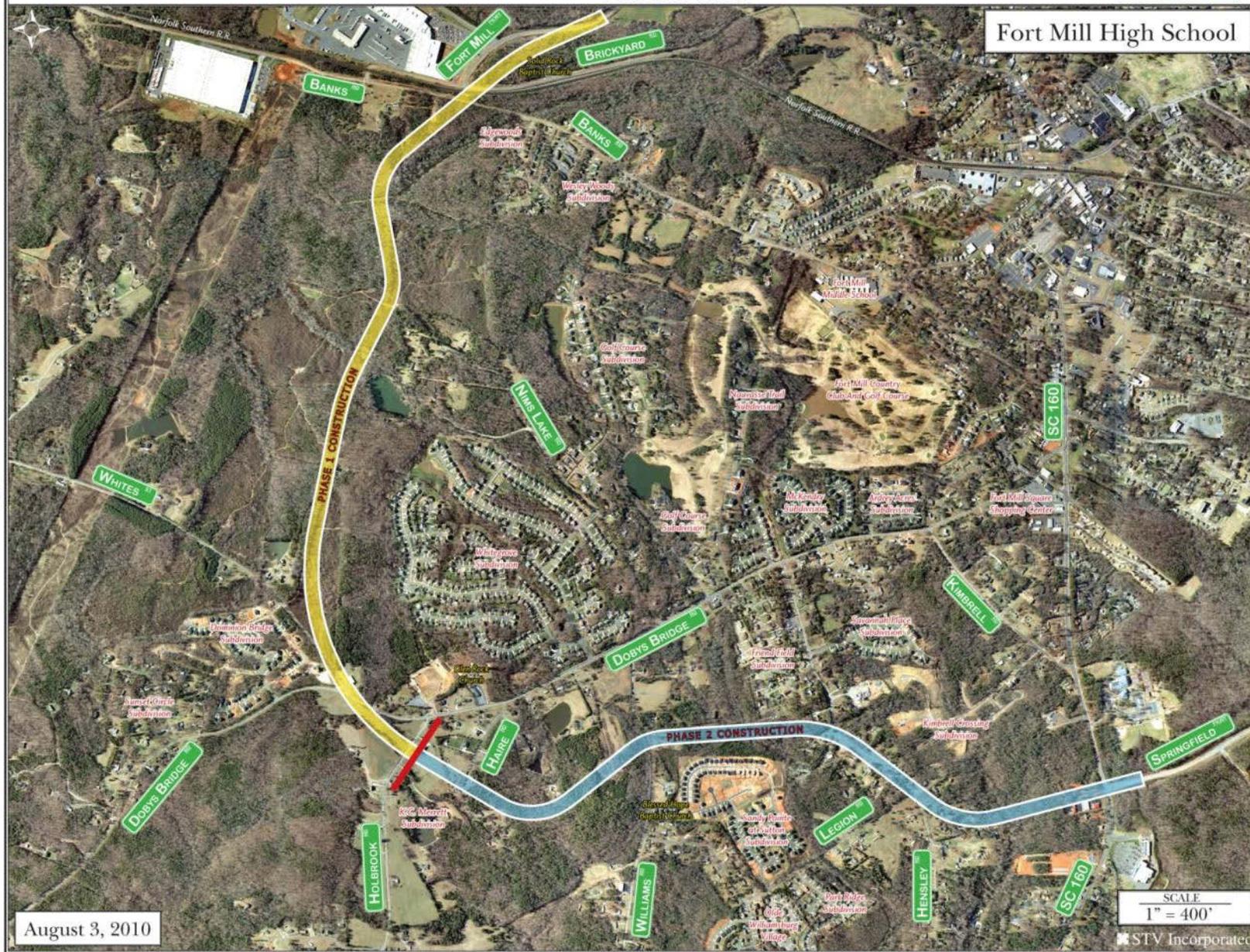
- The proposed development attempts to meet the **intent** of the Corridor Overlay District.
- There are **physical conditions** which prevent the proposed development from meeting the specific standards of this Ordinance.
- The proposed development will be designed to meet the standards of this section to the fullest extent possible.



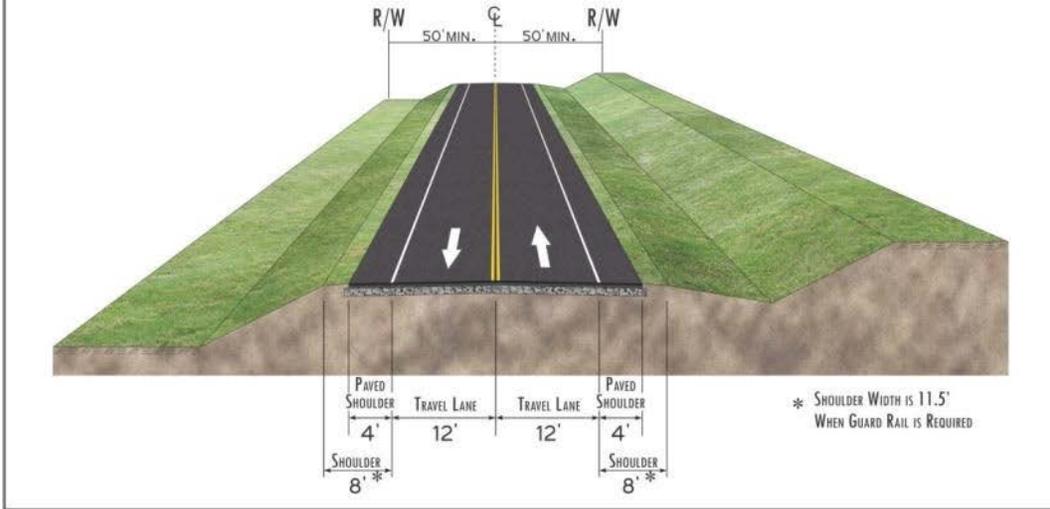
Q & A

A photograph of a park scene. In the foreground, a large tree stands on a paved area with several wooden rocking chairs arranged around its base. In the background, there is a grassy area, a brick wall, and a stop sign. The entire image is overlaid with a semi-transparent green grid pattern. The word "Thanks!" is written in white, bold, sans-serif font across the center of the image.

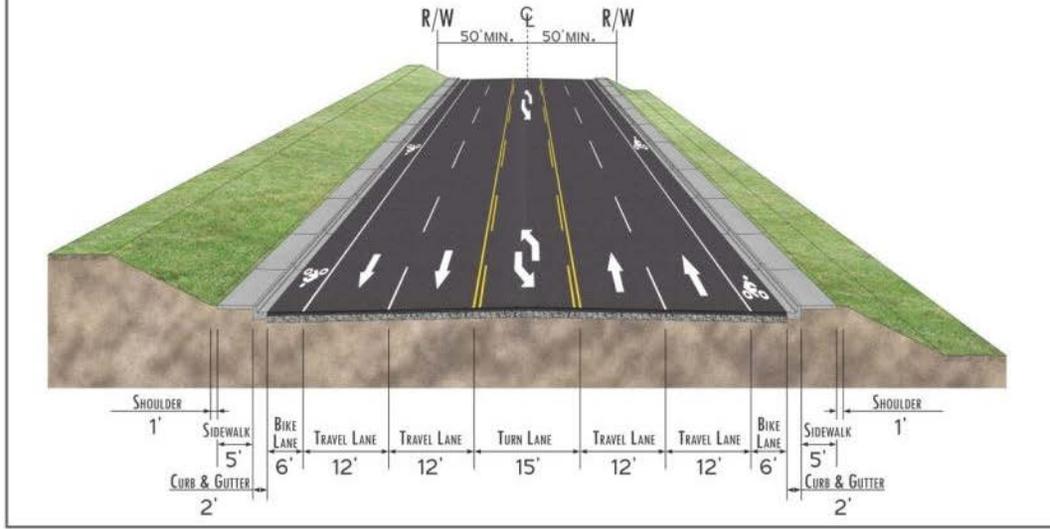
Thanks!



INTERIM DESIGN



ULTIMATE DESIGN



FORT MILL SOUTHERN BYPASS TYPICAL SECTIONS

STV Incorporated





Fort Mill Southern Bypass
near Dobys Bridge Road
Project 03-004
Existing Conditions



Fort Mill Southern Bypass
near Dobys Bridge Road
Project 03-004
Concept Rendering



Fort Mill Southern Bypass
near Legion Road
Project 03-004
Existing Conditions



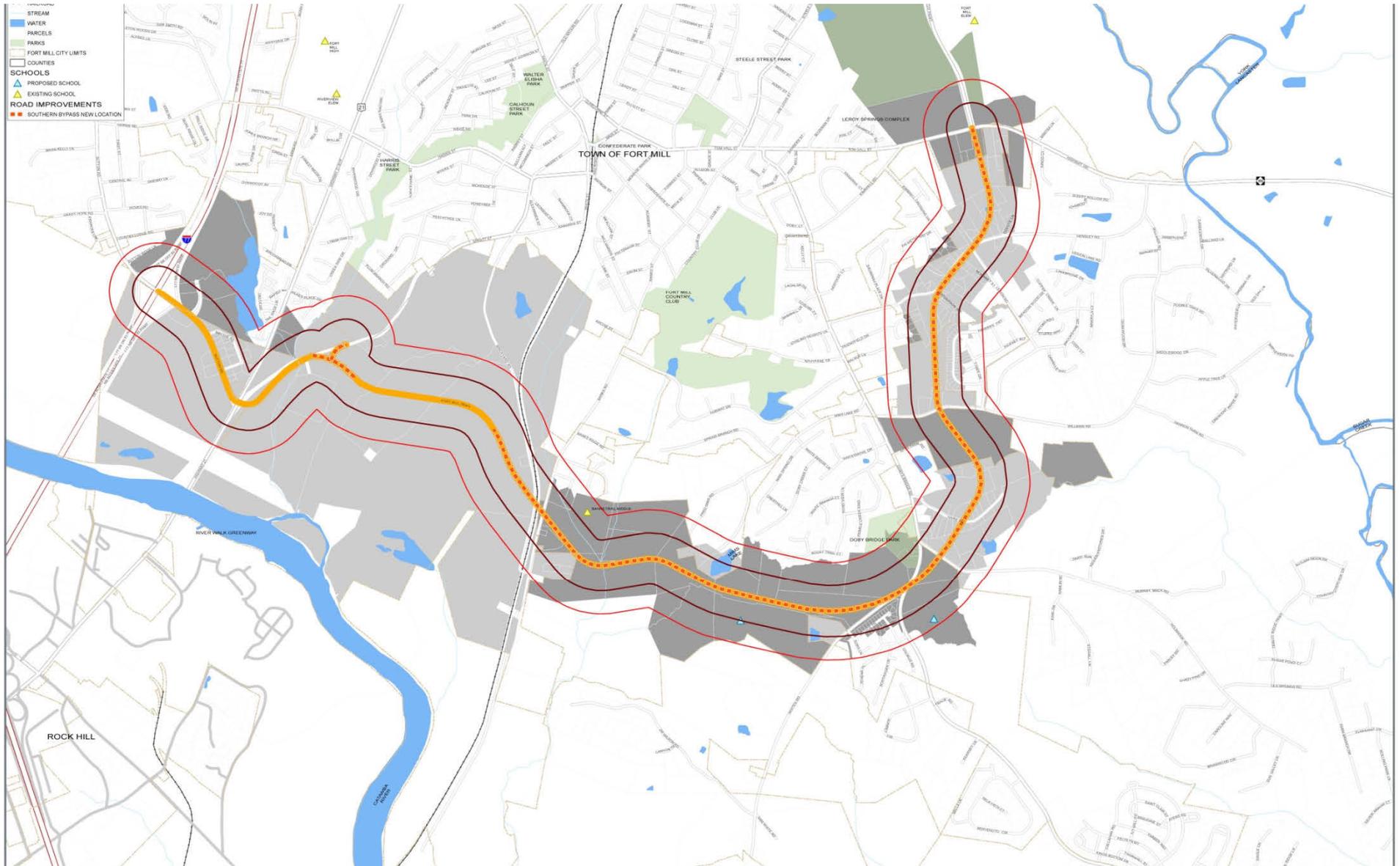
York County
south carolina



Fort Mill Southern Bypass
near Legion Road
Project 03-004

Concept Rendering

Applicability





Design Concept – A



Design Concept – A



Design Concept – B



Design Concept – B